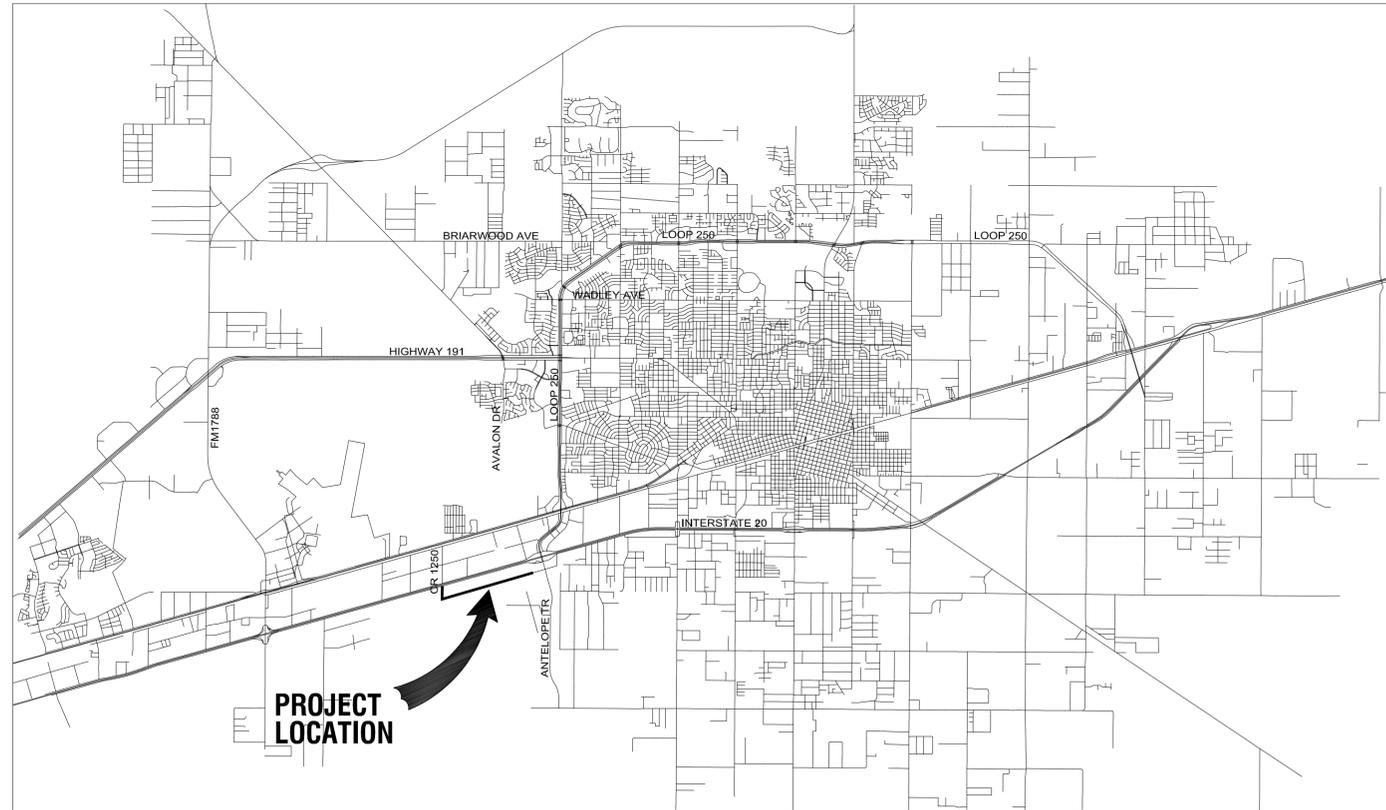


CHOLLA ROAD & COUNTY ROAD 1250 PAVING IMPROVEMENTS MIDLAND, TEXAS



CITY OF MIDLAND
VICINITY MAP
NTS

Parkhill

1700 W. Wall Street
Suite 100
Midland, Texas 79701
432.697.1447

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**Cholla Road and County Road 1250
Paving Improvements**

CLIENT	Midland County	
PROJECT NO.	6000.20	
#	DATE	DESCRIPTION
5	06/08/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Cover Sheet
1

FILE NAME: \\data1\Projects\2020\6000.20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\GEN_01-6000.dwg LAYOUT NAME: 1 Cover Sheet PRINTED: Thursday, June 10, 2021 - 2:44pm USER: K\Ward

FILE NAME: \\data1\Projects\2020\6000.20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\GEN_01+6000.dwg LAYOUT NAME: 2 General Notes PRINTED: Thursday, June 10, 2021 - 2:59pm USER: KWARD

- GENERAL NOTES:**
- DIMENSIONS AND COORDINATES PROVIDED INDICATE THE DESIGN INTENT OF THE ENGINEER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY INCONSISTENCIES OR DISCREPANCIES FOUND DURING CONSTRUCTION. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND COORDINATES DURING CONSTRUCTION LAYOUT PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
 - THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS PRIOR TO AND THROUGHOUT CONSTRUCTION.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION PHASE SURVEYING INCLUDING LOCATING AND VERIFYING PROJECT BENCHMARKS.
 - IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN NEAT AND ACCURATE CONSTRUCTION RECORDS. THE CONTRACTOR SHALL PROVIDE CLEAN AND ACCURATE FULL-SIZE RECORD DRAWINGS WHICH CLEARLY DESCRIBE ANY DEVIATIONS FROM THE PLANS.
 - ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE FOLLOWING, IN ORDER OF PRECEDENCE, (1) DETAILS SHOWN IN THESE PLANS AND SPECIFICATIONS, (2) TEXAS DEPARTMENT OF TRANSPORTATION - "STANDARD SPECIFICATIONS FOR CONSTRUCTION OF HIGHWAY, STREETS AND BRIDGES."
 - IN AREAS WHICH ARE TO REMAIN UNDISTURBED, THE CONTRACTOR SHALL PRESERVE, PROTECT AND/OR RESTORE ALL AREAS DISTURBED BY THE CONSTRUCTION TO ORIGINAL CONDITION OR BETTER AT THE EXPENSE OF THE CONTRACTOR.
 - IN THE EVENT THAT EXISTING PRIVATE UTILITY SERVICES SUCH AS WATER, GAS, TELEPHONE, ELECTRIC, ETC. MUST BE TAKEN OUT OF SERVICE TO FACILITATE CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE TEMPORARY UTILITIES TO THE SATISFACTION OF THE OWNER.
 - THE ENGINEER IS NOT RESPONSIBLE FOR CONSTRUCTION SAFETY.
 - THE LOCATION AND DIMENSIONS SHOWN ON THE PLANS RELATIVE TO EXISTING UTILITIES ARE BASED ON THE BEST RECORDS AND/OR FIELD INFORMATION AVAILABLE. IT IS THE SOLE RESPONSIBILITY OF CONTRACTOR TO VERIFY THE ACCURACY OF SUCH INFORMATION. ALL DAMAGE TO UTILITIES RESULTING FROM CONTRACTOR'S OPERATIONS SHALL BE RESTORED AT EXPENSE OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL PROTECT ALL PROPERTY CORNER MARKERS, AND IF DISTURBED, THEY SHALL BE RESET AT THE EXPENSE OF THE CONTRACTOR.
 - IN THE EVENT THAT OTHER CONTRACTORS ARE DOING WORK IN THE SAME AREA SIMULTANEOUSLY WITH THIS PROJECT, THE CONTRACTOR SHALL COORDINATE HIS PROPOSED CONSTRUCTION WITH THAT OF THE OTHER CONTRACTORS.
 - ALL MATERIALS TO BE REMOVED FROM THE SITE INCLUDING UNSUITABLE SPOIL MATERIAL, REFUSE AND OTHER DEBRIS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE LAWFULLY REMOVED & DISPOSED OF OUTSIDE THE LIMITS OF THE PROJECT.
 - THE CONTRACTOR SHALL MAKE A FINAL CLEAN-UP OF ALL PARTS OF THE WORK AND PREPARE THE SITE IN AN ORDERLY MANNER OF APPEARANCE BEFORE ACCEPTANCE BY THE CITY OF MIDLAND OR MIDLAND COUNTY.
 - HAUL ROADS, ACCESS ROUTES AND THE LOCATION OF ALL STAGING AREAS AND STORAGE AREAS SHALL BE SUBJECT TO THE APPROVAL OF THE CITY OF MIDLAND OR MIDLAND COUNTY.
 - IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE TRAFFIC CONTROL DURING CONSTRUCTION AS REQUIRED BY THE COUNTY AND STATE IN WHICH THE PROJECT IS LOCATED.
 - CONTRACTOR SHALL PREPARE, FURNISH, MAINTAIN, AND REMOVE ALL TRAFFIC CONTROL DEVICES THROUGHOUT CONSTRUCTION. ALL DEVICES SHALL BE IN CONFORMANCE WITH THE TEXAS MUTCD, LATEST EDITION AS CURRENTLY AMENDED BY THE TEXAS DEPARTMENT OF TRANSPORTATION.
 - CONTRACTOR IS RESPONSIBLE FOR PROVIDING A TRAFFIC CONTROL AND SEQUENCING PLAN AND COORDINATING ROAD AND DRIVEWAY CLOSURES WITH THE PROPERTY OWNERS AND AUTHORITIES HAVING JURISDICTION.
 - ALL CONCRETE IS TO BE 3,600 PSI UNLESS OTHERWISE SPECIFIED.

- EROSION CONTROL NOTES:**
- EROSION CONTROL SHEETS ARE INCLUDED IN THE PLAN SET. CONTRACTOR WILL BE RESPONSIBLE FOR PREPARING A STORM WATER POLLUTION PREVENTION PLAN (SWPPP). THE SWPPP IS TO BE KEPT ON-SITE AT ALL TIMES WITH THESE CONSTRUCTION DOCUMENTS AS NECESSARY FOR COMPLIANCE WITH THE TEXAS POLLUTANT DISCHARGE ELIMINATION SYSTEM (TPDES) GENERAL PERMIT.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS AND MAINTAINING COMPLIANCE WITH THE GENERAL PERMIT.
 - THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION, ROUTINE INSPECTION AND/OR MAINTENANCE OF EROSION CONTROL DEVICES.
 - THE EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ACCEPTABLE VEGETATION COVERAGE HAS BEEN ACHIEVED IN ACCORDANCE WITH THE GENERAL PERMIT.
 - ANY ADDITIONAL EROSION CONTROL MEASURES REQUIRED TO COMPLY WITH THE SWPPP OR TCEQ STORMWATER POLLUTION REGULATIONS SHALL BE IMPLEMENTED BY THE CONTRACTOR, AT HIS EXPENSE.
 - DISTURBED AREAS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE CEASED FOR AT LEAST FOURTEEN DAYS SHALL BE TEMPORARILY SEEDED AND WATERED. DISTURBED AREAS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE PERMANENTLY CEASED SHALL BE PERMANENTLY SEEDED/SODDED WITHIN SEVEN DAYS IN ACCORDANCE WITH THE PLANS.
 - IN GENERAL, HYDROSEEDING OR SODDING BERMUDA GRASS IS ACCEPTABLE DURING THE SUMMER MONTHS (MAY 1 - AUGUST 30). WINTER RYE OR FESCUE GRASS MAY BE PLANTED DURING TIMES OTHER THAN THE SUMMER MONTHS AS A TEMPORARY MEASURE UNTIL SUCH TIME AS THE PERMANENT PLANTING CAN BE MADE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR WATERING SEEDED/SODDED AREAS AS NECESSARY UNTIL 70% VEGETATION IS ESTABLISHED IN ACCORDANCE WITH THE PLANS.
 - THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EROSION CONTROL MEASURES ONCE FINAL GROUND STABILIZATION IS ACHIEVED AND THE PROJECT IS COMPLETED.

- GRADING NOTES:**
- THE AREA TO BE GRADED SHOULD BE STRIPPED OF VEGETATION, ROOTS, STUMPS, DEBRIS, AND OTHER ORGANIC MATERIALS.
 - CONSTRUCTION SHALL BE BASED ON SPOT GRADES SHOWN ON THE PLAN AND PROFILE SHEETS. CONTOURS ARE A VISUAL REPRESENTATION OF FINISHED GRADE ONLY AND ARE NOT INTENDED TO BE USED TO SET GRADE.
 - SLOPES ON SITE SHALL NOT EXCEED A 4:1 SLOPE, UNLESS NOTED OTHERWISE.
 - ANY COSTS ASSOCIATED WITH DEWATERING THE SITE SHALL BE DONE AT THE EXPENSE OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL MATCH EXISTING ELEVATIONS AND CONSTRUCT SMOOTH TRANSITIONS AT CONNECTIONS TO EXISTING PAVEMENT AND CURB.

- UTILITY NOTES:**
- CONTRACTOR IS RESPONSIBLE FOR ADJUSTING VALVE BOXES PER DETAILS 608(A&B), SHEET 35. PAYMENT WILL BE PER THE BID ITEM.
 - REPORT ANY UTILITY CONFLICTS TO THE ENGINEER IMMEDIATELY AND NOTIFY THE RESPECTIVE UTILITY COMPANY.
 - THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN AREAS ADJACENT TO GAS LINES, UNDERGROUND ELECTRIC CABLE, FIBER OPTIC CABLE AND UNDERGROUND TELEPHONE CABLE.
 - WHERE EXISTING UTILITIES OR SERVICE LINES ARE CUT, BROKEN OR DAMAGED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER OF THE RESPECTIVE UTILITY. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING OR REPAIRING THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER, UNLESS OTHERWISE SHOWN OR NOTED ON THE PLANS. THE CONTRACTOR SHALL ALSO NOTIFY THE ENGINEER OF ANY CONFLICTS IN GRADES AND ALIGNMENT.

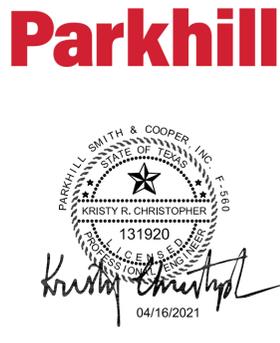
- TRAFFIC CONTRAO PLAN NOTES:**
- ROAD IS TO BE CLOSED TO PUBLIC DURING CONSTRUCTION. CONTRACTOR MAY SUBMIT TRAFFIC CONTROL PLAN TO THE CITY OF MIDLAND AND MIDLAND COUNTY FOR APPROVAL IF ALTERNATE TRAFFIC CONTROL PLAN IS DESIRED.
 - TYPICAL TXDOT TRAFFIC CONTROL PLAN SHEET INCLUDED FOR CONTRACTOR CONVENIENCE. CONTRACTOR WHOLLY RESPONSIBLE FOR IMPLEMENTING A TCF.

- DEMOLITION NOTES:**
- THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSING OF EXISTING STRUCTURES, UTILITIES, PAVEMENT, TREES, ETC., WITHIN CONSTRUCTION LIMITS AS SHOWN ON PLANS, IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES AT CONTRACTOR'S EXPENSE. AREAS WHERE MATERIAL HAS BEEN REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT BACK UP TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL IN ACCORDANCE WITH GEOTECHNICAL REPORT.
 - THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
 - PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.
 - THE CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES.
 - CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING FACILITIES.
 - CONTRACTOR MAY LIMIT SAWCUT AND PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS, BUT IF ANY DAMAGE IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR.
 - ALL FENCES REMOVED TO FACILITATE CONSTRUCTION SHALL BE REPLACED AT THE EXISTING OR PROPOSED LOCATION AS DIRECTED BY THE CITY OF MIDLAND AND MIDLAND COUNTY.

- PAVING NOTES:**
- THE CONTRACTOR SHALL PROVIDE A FULL DEPTH SAW-CUT AND SMOOTH TRANSITION AT CONNECTIONS TO EXISTING PAVEMENT AND CURB.
 - THE CONTRACTOR SHALL NOT STAND, PARK, DRIVE ON, OR IN ANY WAY DISTURB OR DAMAGE STEEL REINFORCING FOR SITE WORK. ALL REINFORCING SHALL BE INSTALLED WITH CHAIRS PER THE PLANS AND SPECIFICATIONS.
 - MAINTAIN SUBGRADE SOIL MOISTURE AND DENSITY PRIOR TO PLACING THE BASE COURSE. MAINTAIN SUBGRADE SOIL MOISTURE TO NO LESS THAN 4% BELOW OPTIMUM, OR AS APPROVED BY THE ENGINEER. REWORK AS DIRECTED BY THE ENGINEER.
 - ALL EXPANSION JOINTS SHALL BE CONSTRUCTED OF REDWOOD.
 - REFERENCE SITE PLAN FOR PAVING DIMENSIONS AND LAYOUT.
 - CONTRACTOR SHALL AVOID CONSTRUCTING IRREGULAR SHAPED PANELS. AN IRREGULAR SHAPED PANEL IS CONSIDERED TO BE ONE IN WHICH THE SLAB TAPERS TO A SHARP ANGLE, WHEN THE LENGTH TO WIDTH RATIO EXCEEDS 3 TO 1, OR WHEN A SLAB IS NEITHER SQUARE NOR RECTANGULAR.
 - MAINTAIN FLEXIBLE BASE MOISTURE AND DENSITY PRIOR TO PLACING THE PRIME COAT. MAINTAIN FLEXIBLE BASE MOISTURE TO NO LESS THAN 4% BELOW OPTIMUM, OR AS APPROVED BY THE ENGINEER. REWORK AS DIRECTED BY THE ENGINEER.
 - ALLOW PRIME TO CURE FOR A MINIMUM OF 72 HOURS PRIOR TO PLACING FINAL HMAC SURFACE, OR AS ALLOWED BY THE ENGINEER. REMOVE AND REPLACE ANY HMAC PLACED PRIOR TO THE CURING TIME REQUIRED.

- NO RAP ALLOWED IN THE TYPE C HMAC SURFACE COURSE.
 - COMPACTION TESTS WILL BE MADE ON THE ASPHALTIC SURFACING ON A LOT BASIS USING THE CORES OBTAINED FOR THICKNESS DETERMINATION. THE PERCENT DENSITY FOR ACCEPTANCE OF A LOT WILL BE CALCULATED USING MAXIMUM THEORETICAL SPECIFIC GRAVITY OF THE PLACED MIXTURE. ASPHALTIC SURFACING NOT MEETING THESE SPECIFICATIONS SHALL BE REMOVED AND REPLACED.
- | AVERAGE PERCENT DENSITY OF A LOT | PERCENT OF CONTRACT UNIT PRICE TO BE PAID |
|----------------------------------|---|
| 97.4 AND GREATER | REMOVE AND REPLACE |
| 90.1 - 97.3 | 100 |
| LESS THAN 90.1 | REMOVE AND REPLACE |

- PAVEMENT MARKINGS AND SIGN ASSEMBLIES NOTES:**
- COORDINATE STRIPING LAYOUT WITH THE ENGINEER, TXDOT, THE CITY OF MIDLAND, AND MIDLAND COUNTY PRIOR TO FINAL INSTALLATION OF STRIPES, SYMBOLS, STOP BARS, AND SIGNS.



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Cholla Road and County Road 1250
Paving Improvements

CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

FILE NAME: A:\2020\6000_2003_DSGN01_DWG\050_CIVIL\00_SHEETS\CORD_CONT_01+6000.dwg LAYOUT NAME: 4 Coordinate Control Plan PRINTED: Wednesday, March 03, 2021 - 11:44pm USER: KChristopher

LEGEND

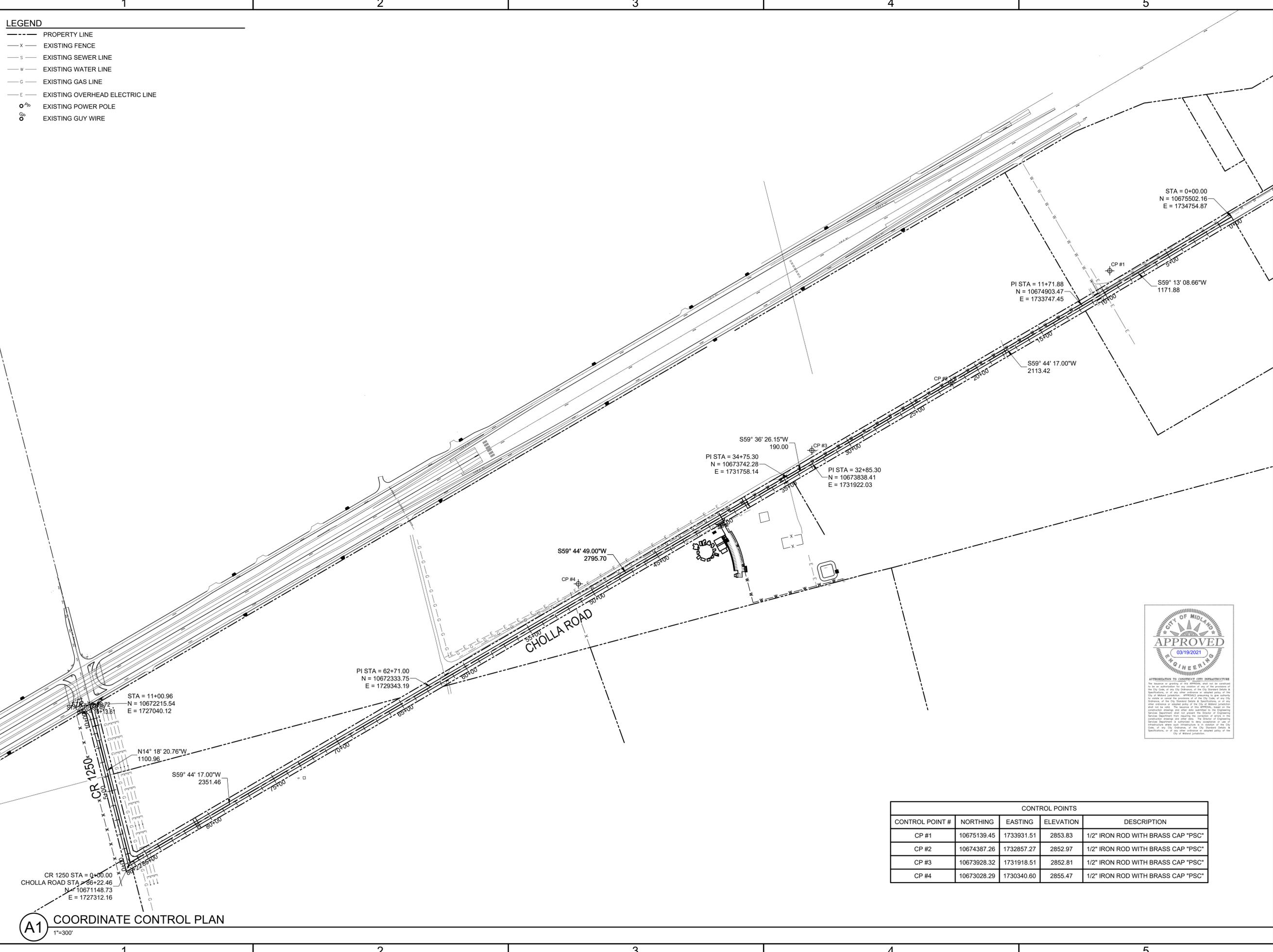
---	PROPERTY LINE
-X-	EXISTING FENCE
-S-	EXISTING SEWER LINE
-W-	EXISTING WATER LINE
-G-	EXISTING GAS LINE
-E-	EXISTING OVERHEAD ELECTRIC LINE
○	EXISTING POWER POLE
○	EXISTING GUY WIRE

D

C

B

A



CONTROL POINTS				
CONTROL POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
CP #1	10675139.45	1733931.51	2853.83	1/2" IRON ROD WITH BRASS CAP "PSC"
CP #2	10674387.26	1732857.27	2852.97	1/2" IRON ROD WITH BRASS CAP "PSC"
CP #3	10673928.32	1731918.51	2852.81	1/2" IRON ROD WITH BRASS CAP "PSC"
CP #4	10673028.29	1730340.60	2855.47	1/2" IRON ROD WITH BRASS CAP "PSC"

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Cholla Road and County Road 1250
Paving Improvements



CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Coordinate Control Plan

A1 COORDINATE CONTROL PLAN
1"=300'

FILE NAME: A:\2020\6000_2003_DSGN01_DWG\050_CIVIL\00_SHEETS\DEMO-6000.dwg LAYOUT NAME: 5 Demolition Plan PRINTED: Wednesday, March 03, 2021 - 11:45pm USER: KChristopher



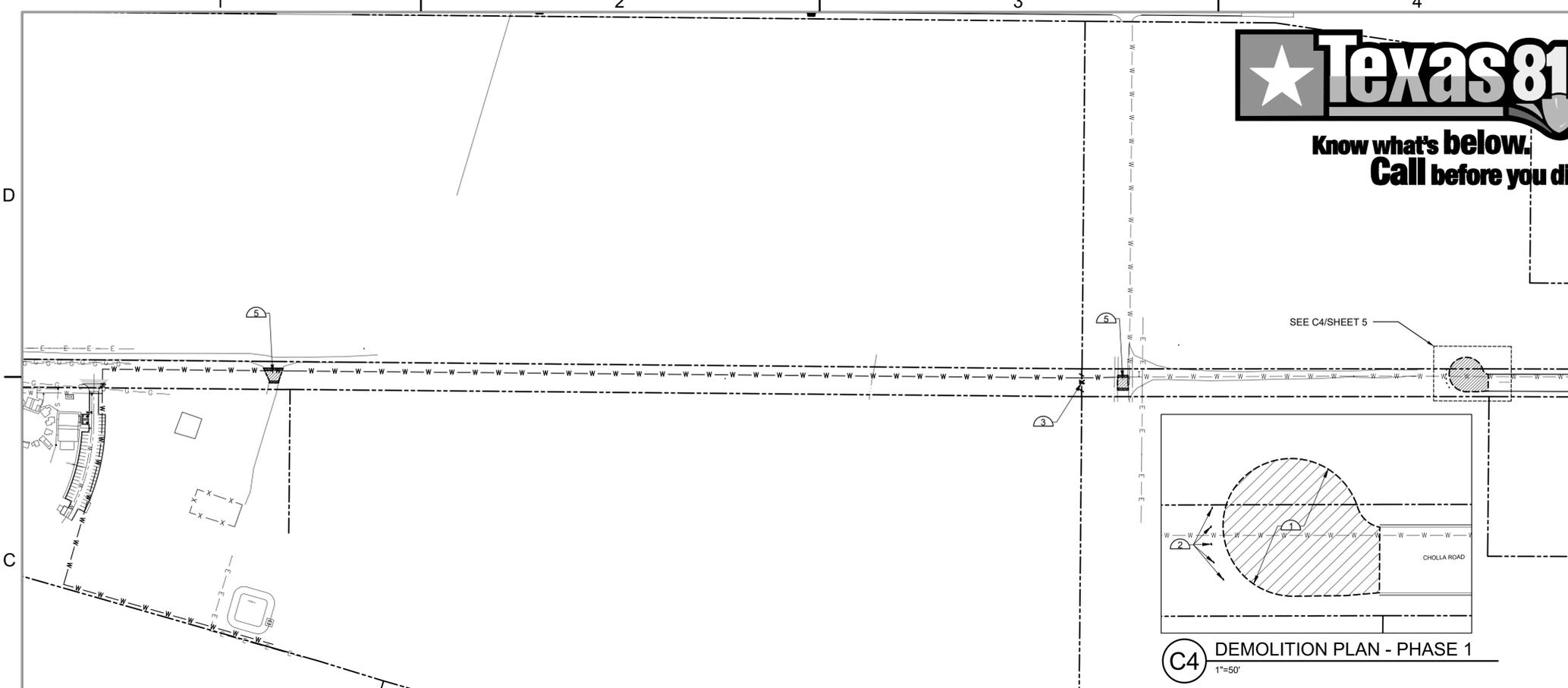
CONTRACTOR IS RESPONSIBLE TO CONTACT UTILITIES & ONE CALL SYSTEM 14 DAYS PRIOR TO DIGGING
ALL SECTIONS DIGTESS @ 1-800-344-8377

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Cholla Road and County Road 1250
Paving Improvements

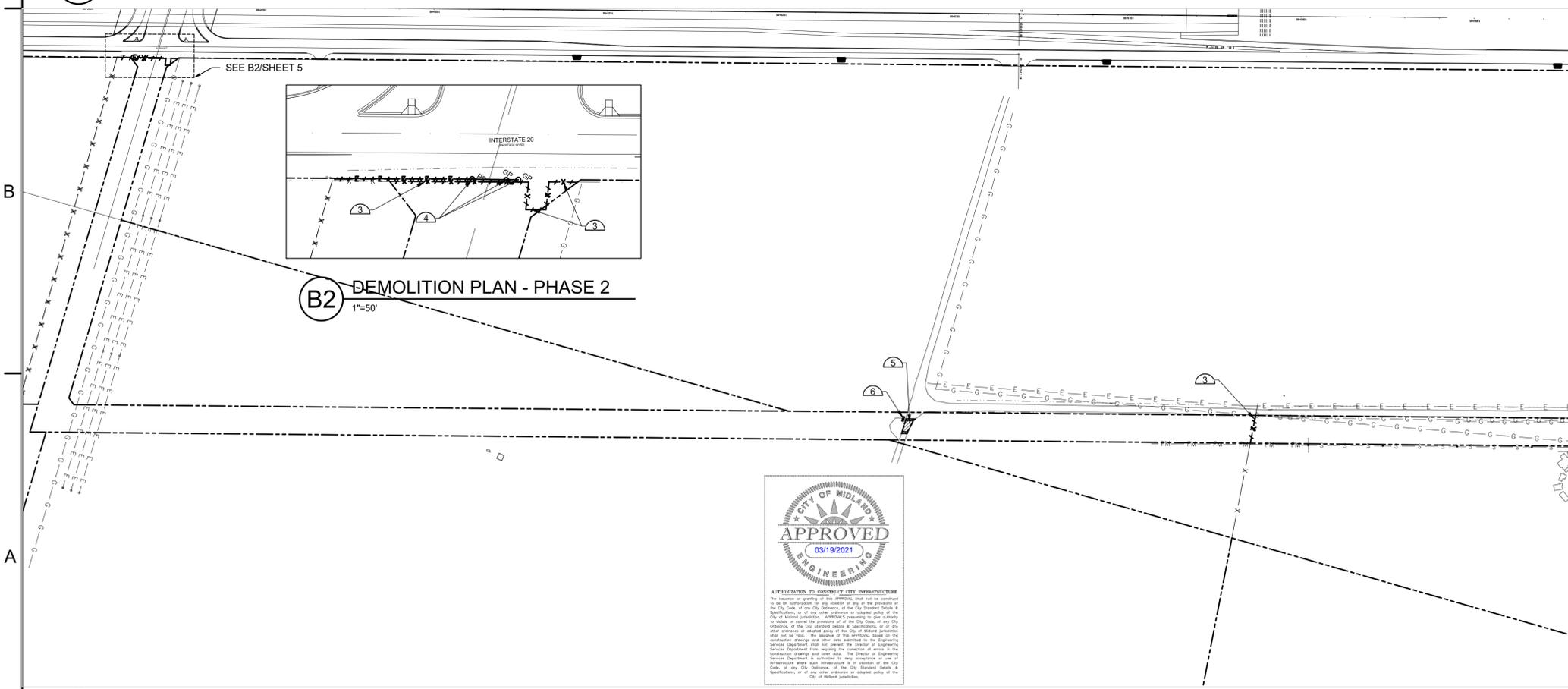


C1 DEMOLITION PLAN - PHASE 1
1"=200'

C4 DEMOLITION PLAN - PHASE 1
1"=50'

DEMOLITION NOTES

- 1 REMOVE EXISTING ASPHALT PAVEMENT AND FLEXIBLE BASE
- 2 REMOVE EXISTING END OF ROADWAY SIGNAGE
- 3 REMOVE EXISTING FENCE WITHIN THE ROW EXTENTS
- 4 COORDINATE WITH ONCOR TO REMOVE AND RELOCATE EXISTING ELECTRICAL POWER POLE AND GUY WIRES
- 5 REMOVE EXISTING CALICHE ROAD TO EXTENTS OF NEW ROADWAY
- 6 COORDINATE THE RELOCATION OF THE EXISTING WATER PIPELINE RISER TO OUTSIDE OF ROW WITH THE OWNER



B2 DEMOLITION PLAN - PHASE 2
1"=50'

A1 DEMOLITION PLAN - PHASE 2
1"=200'

NOTES

- A. THE EXISTING UTILITIES, ABOVE GROUND AND UNDER GROUND, INDICATED ON THESE PLANS HAVE BEEN LOCATED FROM REFERENCE INFORMATION SUPPLIED BY OTHERS. VERIFY, BOTH HORIZONTALLY AND VERTICALLY, THE LOCATIONS OF ALL EXISTING UTILITIES, APPURTENANCES, OR OTHER FEATURES, PRIOR TO CONSTRUCTION. TAKE ALL NECESSARY PRECAUTIONS TO PROTECT ALL EXISTING UTILITIES, APPURTENANCES, AND ANY OTHER FEATURES ENCOUNTERED, AND NOTIFY THE ENGINEER PROMPTLY OF ANY CONFLICTS WITH THE WORK.
- B. CONTACT THE ONE CALL SYSTEM (811) PRIOR TO CONSTRUCTION. REPAIR DAMAGE TO ANY EXISTING UTILITIES AND FACILITIES.
- C. HAUL AWAY AND PROPERLY DISPOSE OF ALL MATERIAL REMOVED/DEMOLISHED FROM THE SITE IN ACCORDANCE WITH CITY, STATE, AND NATIONAL REQUIREMENTS.
- D. CONTRACTOR IS RESPONSIBLE FOR ALL MISCELLANEOUS REPAIRS DUE TO DEMOLITION DAMAGE, AT HIS OWN EXPENSE.

LEGEND

- DEMOLISH EXISTING ASPHALT OR HARD SURFACE AND REMOVE FROM SITE
- CHAIN-LINK FENCING TO BE REMOVED
- PROPERTY LINE
- EXISTING FENCE
- EXISTING SEWER LINE
- EXISTING WATER LINE
- EXISTING GAS LINE
- EXISTING OVERHEAD ELECTRIC LINE
- EXISTING POWER POLE
- EXISTING GUY WIRE



CLIENT
Midland County

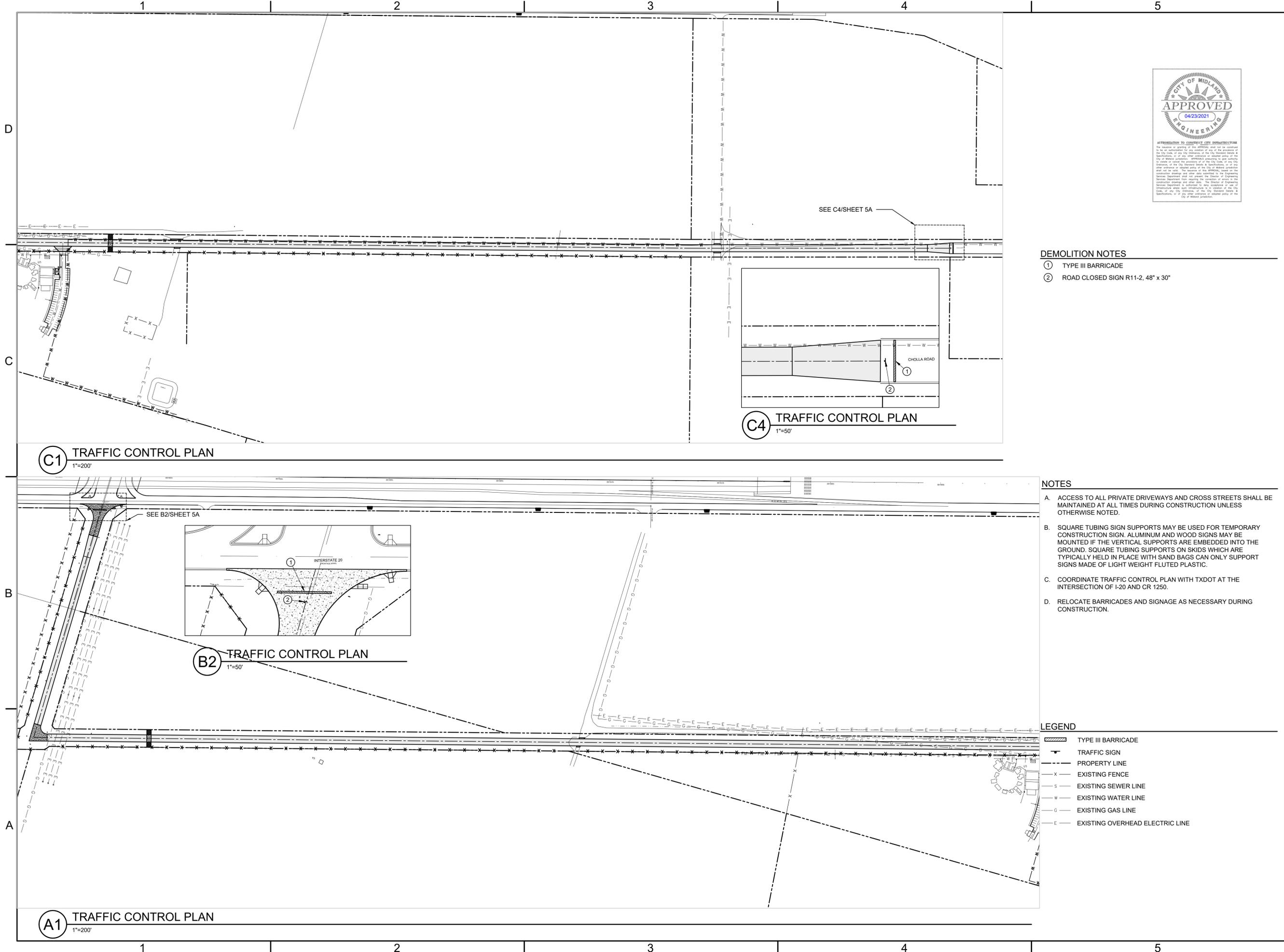
PROJECT NO.
6000.20

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Demolition Plan

5

FILE NAME: \\Data1\Projects\3\2020\6000.20\03_DSGN\01_DWG\050_CIV\00_SHEETS\TCP-6000.dwg LAYOUT NAME: 5A Traffic Control Plan PRINTED: Friday, April 16, 2021 - 4:42pm USER: KChristopher



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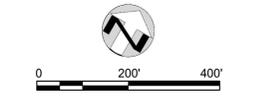
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Cholla Road and County Road 1250
Paving Improvements

- DEMOLITION NOTES**
- ① TYPE III BARRICADE
 - ② ROAD CLOSED SIGN R11-2, 48" x 30"

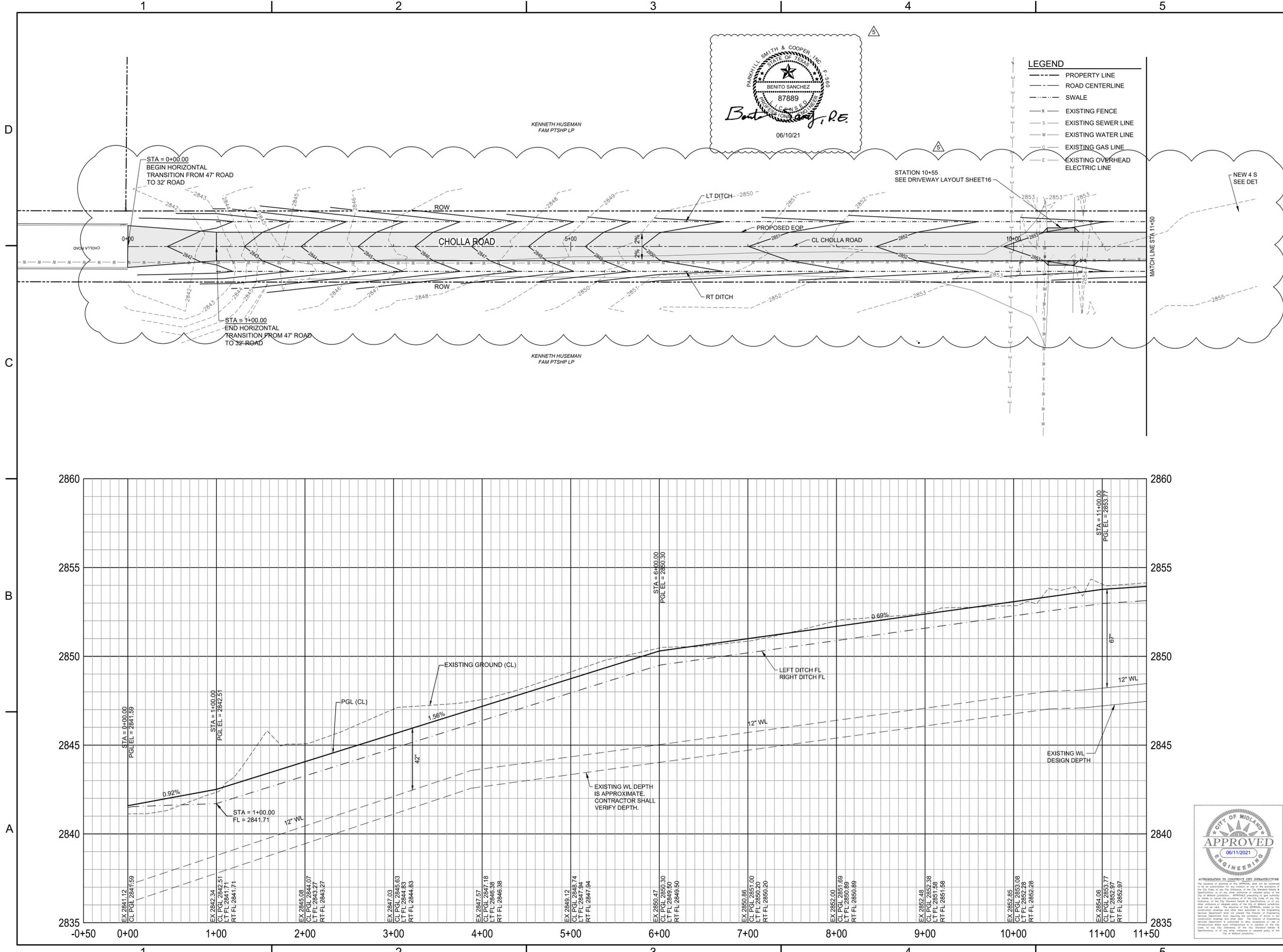
- NOTES**
- A. ACCESS TO ALL PRIVATE DRIVEWAYS AND CROSS STREETS SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION UNLESS OTHERWISE NOTED.
 - B. SQUARE TUBING SIGN SUPPORTS MAY BE USED FOR TEMPORARY CONSTRUCTION SIGN. ALUMINUM AND WOOD SIGNS MAY BE MOUNTED IF THE VERTICAL SUPPORTS ARE EMBEDDED INTO THE GROUND. SQUARE TUBING SUPPORTS ON SKIDS WHICH ARE TYPICALLY HELD IN PLACE WITH SAND BAGS CAN ONLY SUPPORT SIGNS MADE OF LIGHT WEIGHT FLUTED PLASTIC.
 - C. COORDINATE TRAFFIC CONTROL PLAN WITH TXDOT AT THE INTERSECTION OF I-20 AND CR 1250.
 - D. RELOCATE BARRICADES AND SIGNAGE AS NECESSARY DURING CONSTRUCTION.

- LEGEND**
- ▨ TYPE III BARRICADE
 - ⊠ TRAFFIC SIGN
 - - - PROPERTY LINE
 - x-x- EXISTING FENCE
 - s-s- EXISTING SEWER LINE
 - w-w- EXISTING WATER LINE
 - g-g-g- EXISTING GAS LINE
 - e-e-e- EXISTING OVERHEAD ELECTRIC LINE



CLIENT	
Midland County	
PROJECT NO.	
6000.20	
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4	04/16/2021 Midland County Comments
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Traffic Control Plan
5A

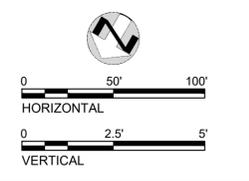


LEGEND

- PROPERTY LINE
- ROAD CENTERLINE
- SWALE
- X- EXISTING FENCE
- S- EXISTING SEWER LINE
- W- EXISTING WATER LINE
- G- EXISTING GAS LINE
- E- EXISTING OVERHEAD ELECTRIC LINE



Cholla Road and County Road 1250
Paving Improvements



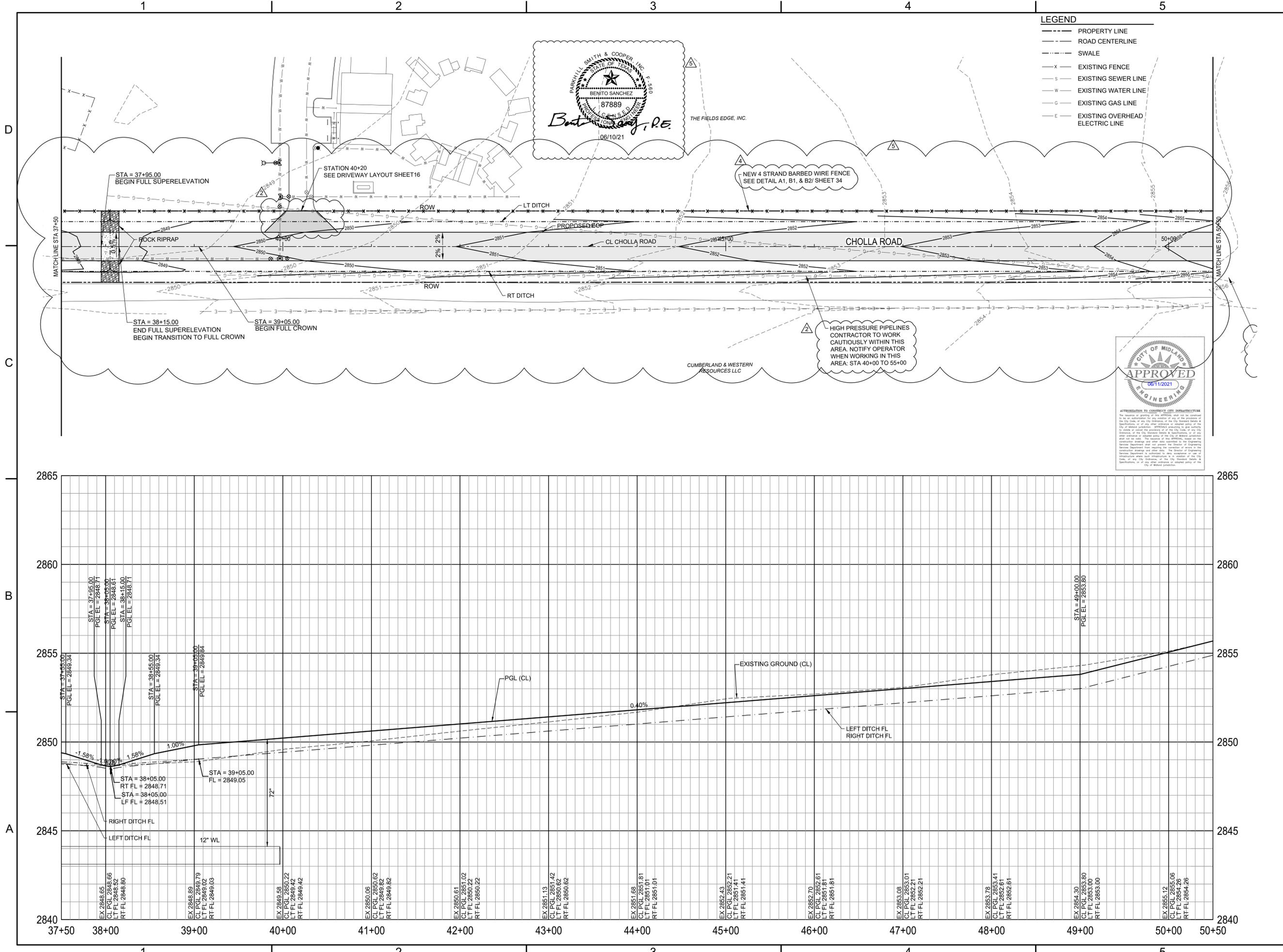
CLIENT
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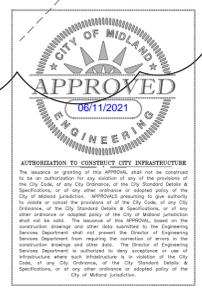
#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
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Cholla Road Plan And Profile

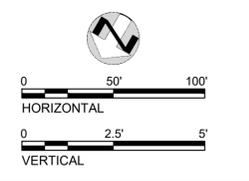


- LEGEND**
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Cholla Road and County Road 1250
Paving Improvements



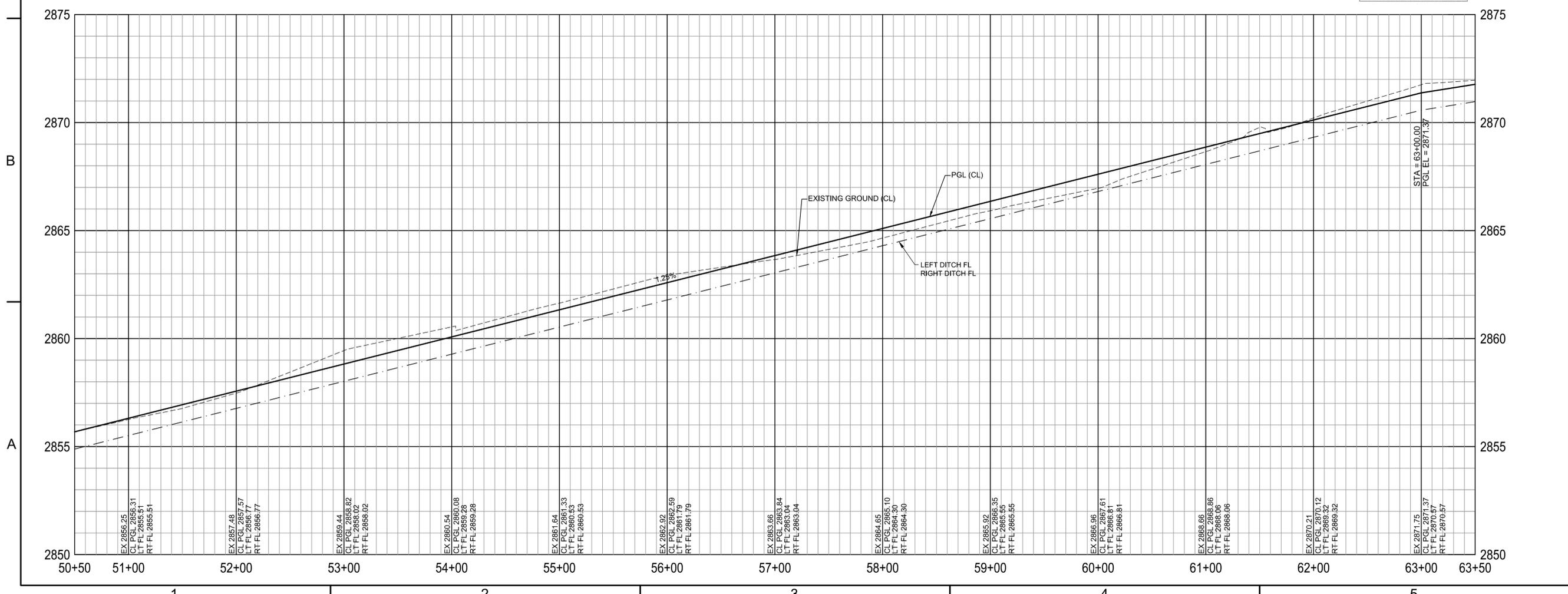
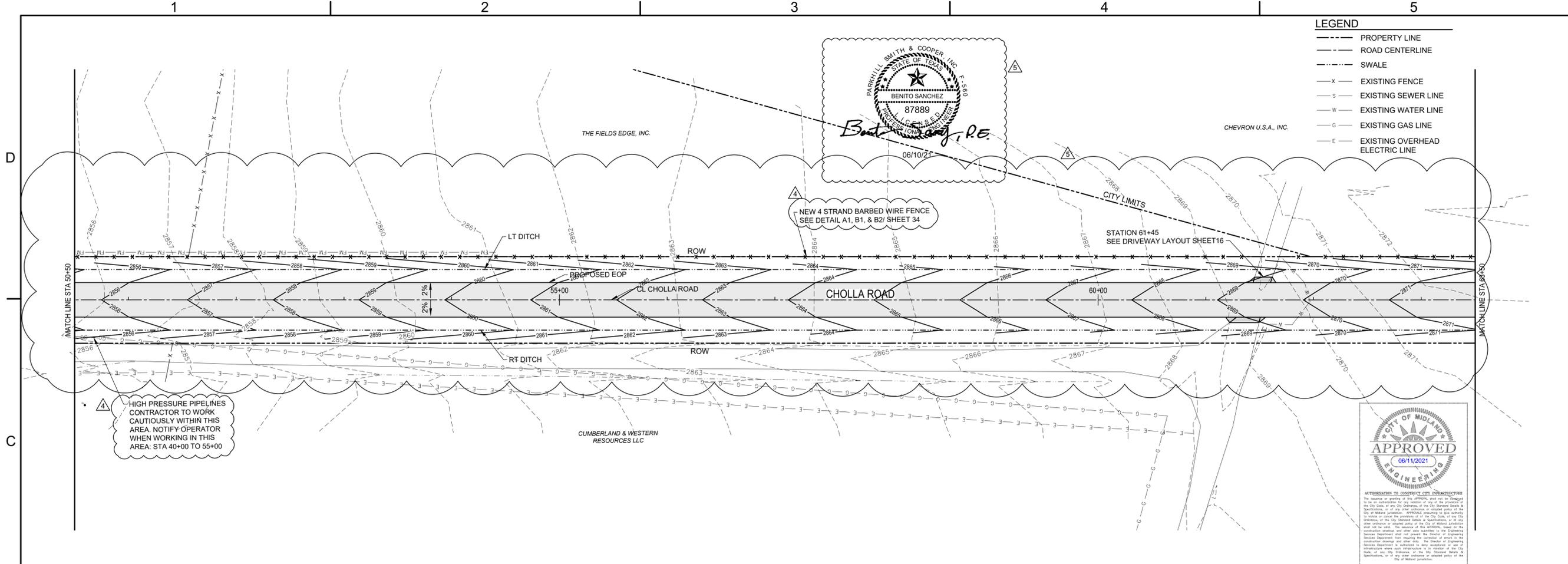
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Cholla Road Plan And Profile

FILE NAME: A:\2020\6000_2003_DSGN01_DWG\050_CIVIL\00_SHEETS\PAVE_01+6000.dwg LAYOUT NAME: 10 Cholla Road Plan and Profile - Friday, June 11, 2021 - 3:41am USER: BSanchez

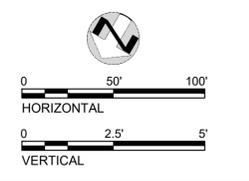


Parkhill



Parkhill.com

Cholla Road and County Road 1250
Paving Improvements

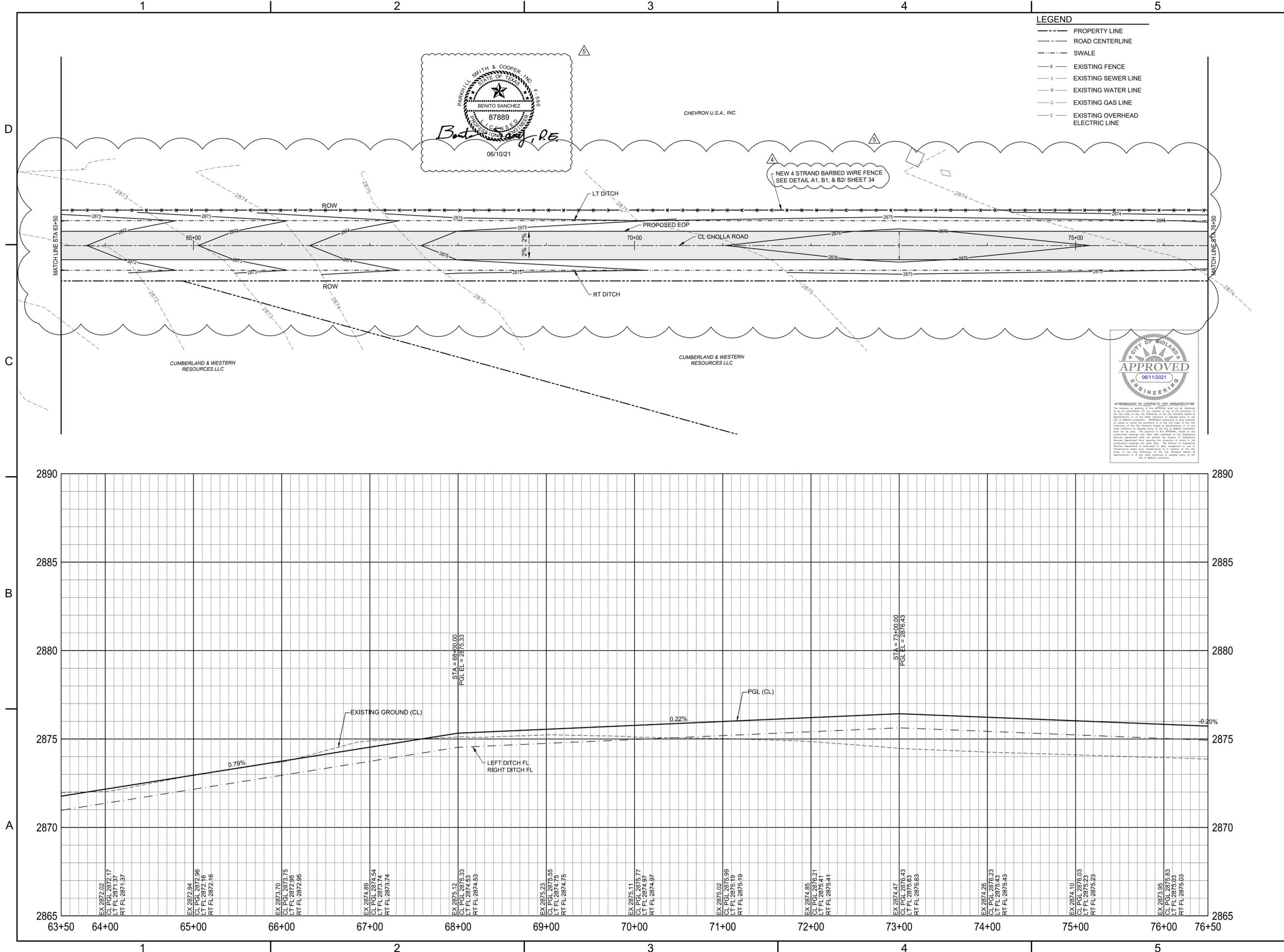


CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

FILE NAME: A:\2020\6000\2003_DSGN01_DWG\050_CIVIL\00_SHEETS\PAVE_01+6000.dwg LAYOUT NAME: 11 Cholla Road Plan and Profile - Friday, June 11, 2021 - 3:43am USER: BSanchez



- LEGEND**
- PROPERTY LINE
 - ROAD CENTERLINE
 - SWALE
 - x- EXISTING FENCE
 - s- EXISTING SEWER LINE
 - w- EXISTING WATER LINE
 - g- EXISTING GAS LINE
 - e- EXISTING OVERHEAD ELECTRIC LINE



CHEVRON U.S.A., INC.

NEW 4 STRAND BARBED WIRE FENCE
SEE DETAIL A1, B1, & B2/ SHEET 34

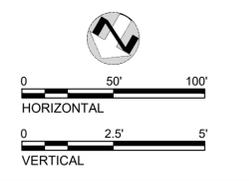


Parkhill



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Cholla Road and County Road 1250
Paving Improvements



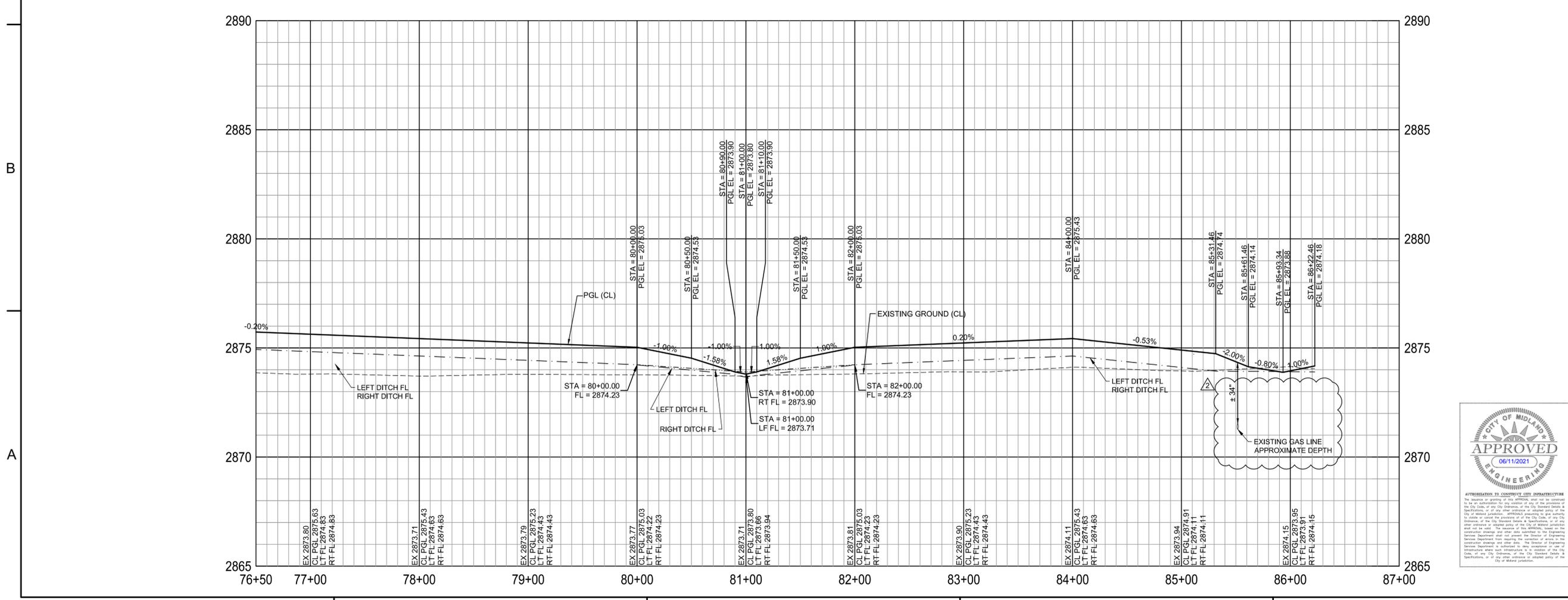
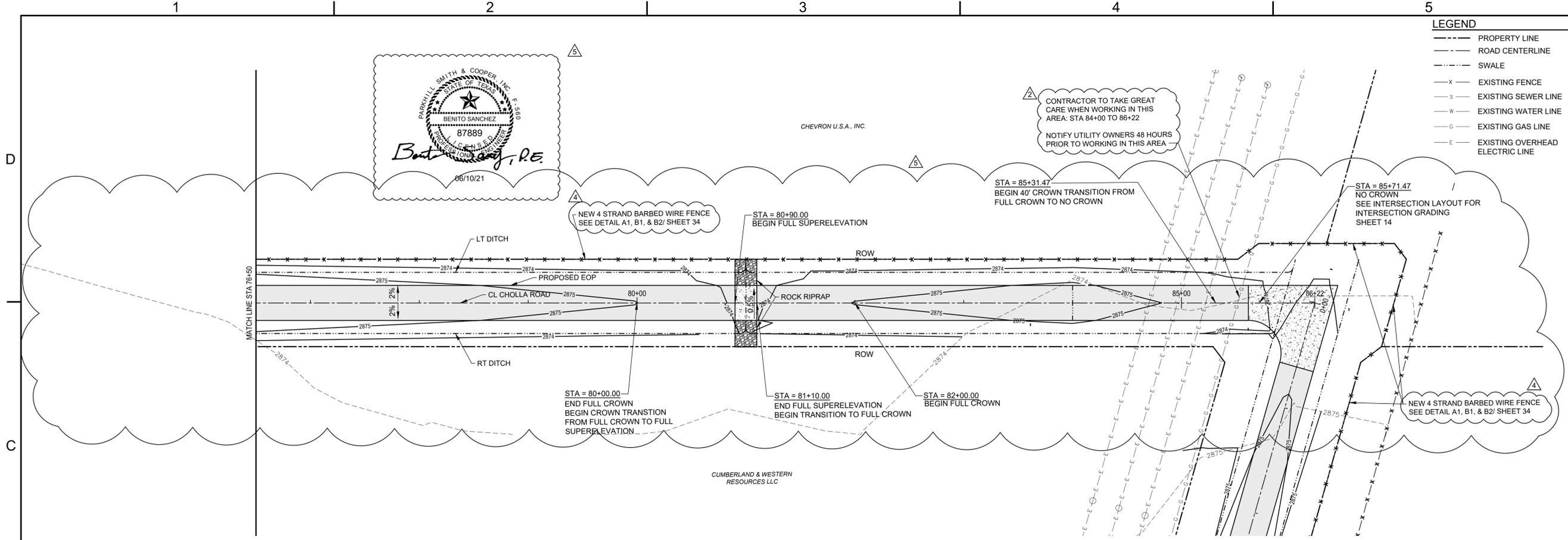
CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Cholla Road Plan And Profile

FILE NAME: A:\2020\6000.2003_DSGN01_DWG\050_CIVIL\00_SHEETS\PAVE_01+6000.dwg LAYOUT NAME: 12 Cholla Road Plan and Profile - Friday, June 11, 2021 - 3:44am USER: BSanchez

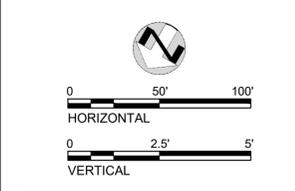


Parkhill

PARKHILL SMITH & COOPER, INC. F.S. 509
 KRISTY R. CHRISTOPHER
 131920
 04/16/2021

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Cholla Road and County Road 1250
 Paving Improvements



CLIENT
 Midland County

PROJECT NO.
 6000.20

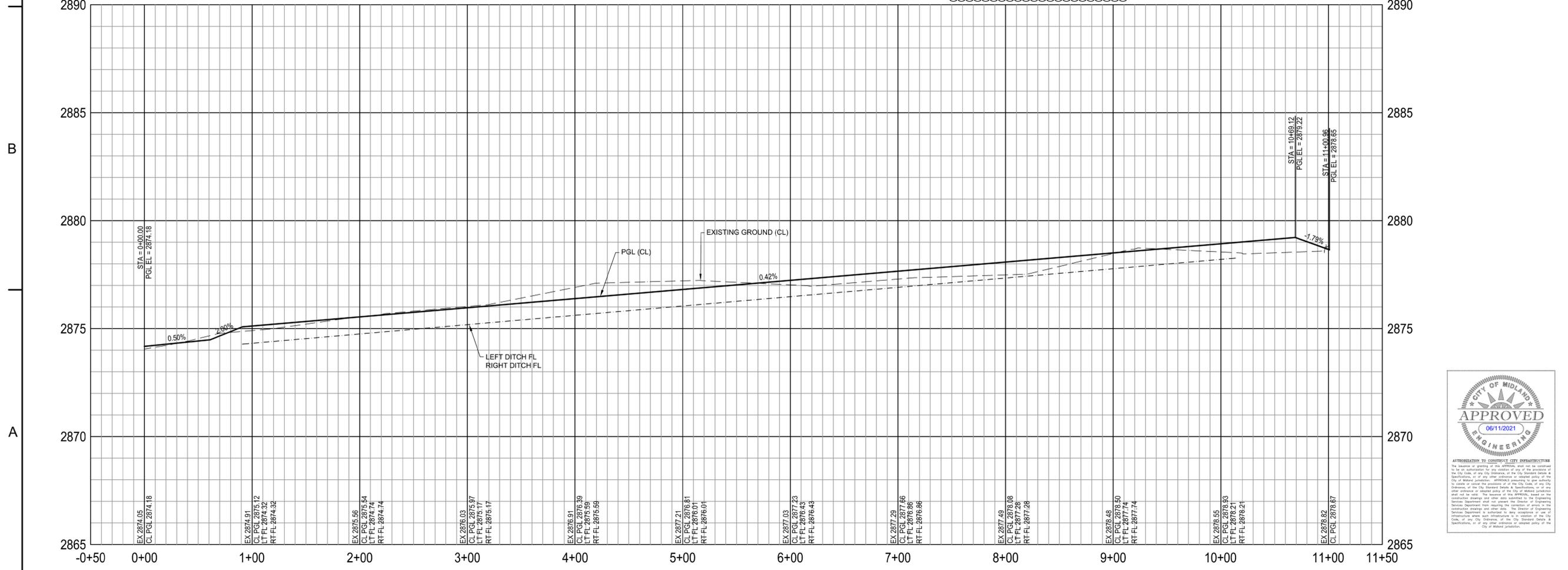
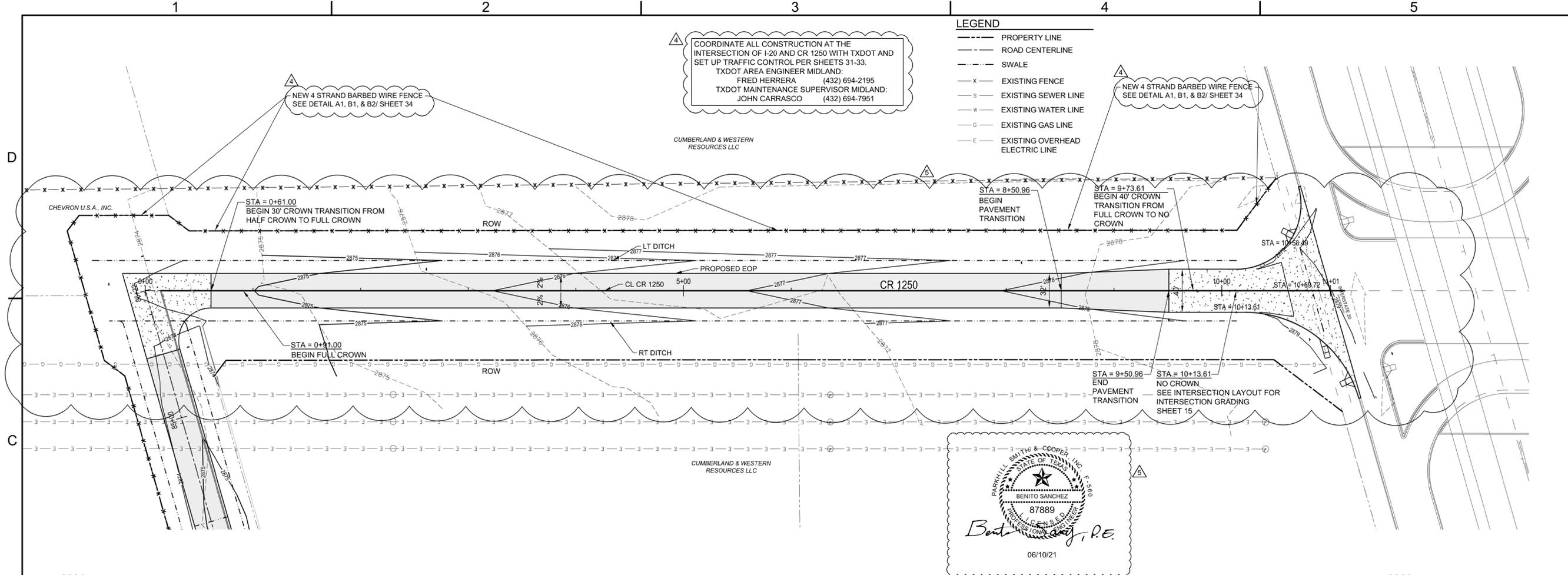
#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection



Cholla Road Plan And Profile

12

FILE NAME: A:\2020\6000_20103_DSGN\01_DWG\050_CIVIL\00_SHEETS\PAVE_02-6000.dwg LAYOUT NAME: 13 CR 1250 Road Plan and Profile PRINTED: Friday, June 11, 2021 - 8:22am USER: K\Ward

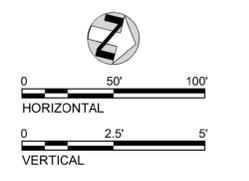


Parkhill



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Cholla Road and County Road 1250
Paving Improvements



CLIENT
Midland County

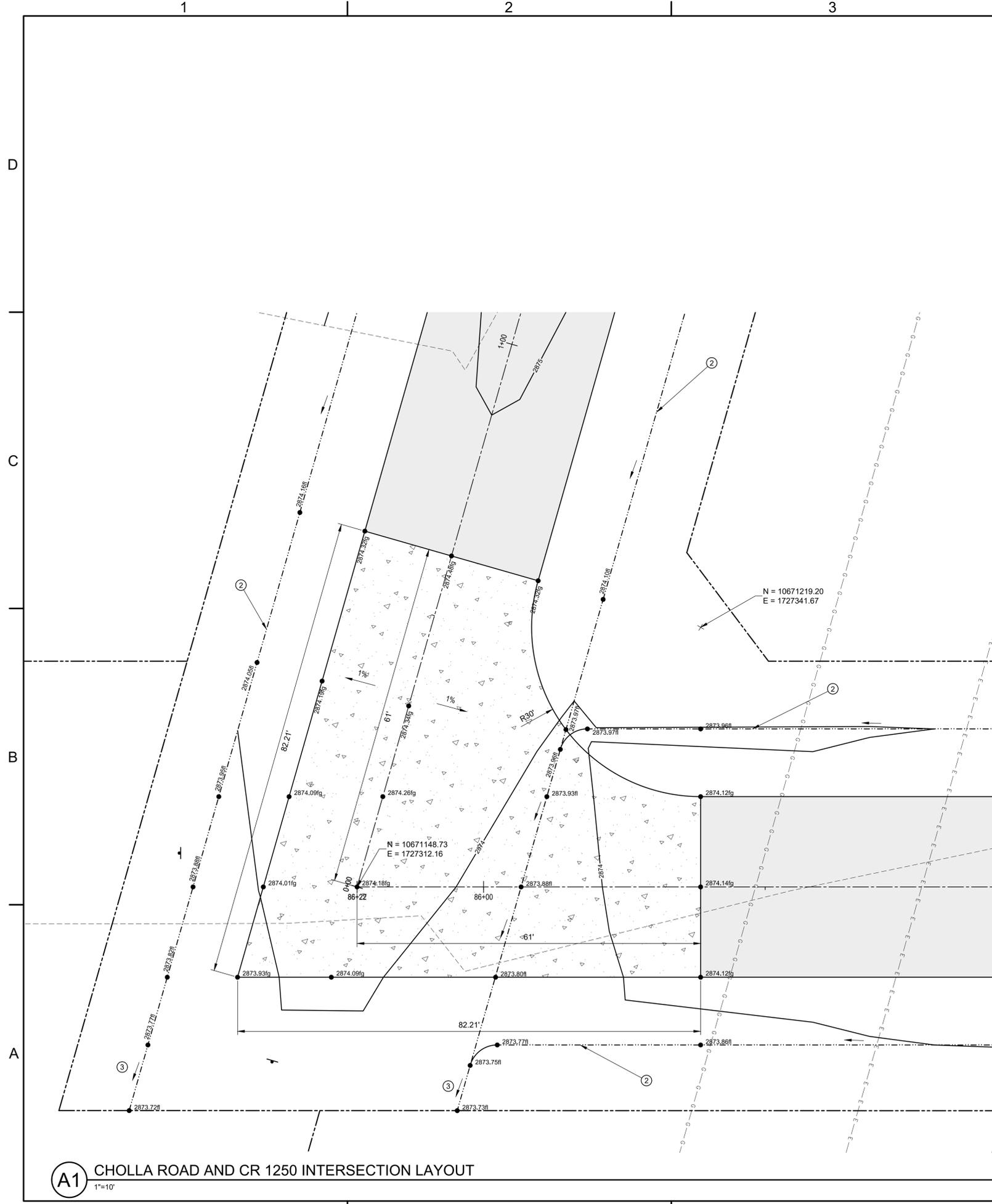
PROJECT NO.
6000.20

#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection



CR 1250 Road
Plan And Profile

FILE NAME: A:\2020\6000_20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\BLOWUPS-6000.dwg LAYOUT NAME: 14 Cholla Road and CR 1250 Intersection Layout PRINTED: Wednesday, March 17, 2021 - 10:57am USER: KChristopher



A1 CHOLLA ROAD AND CR 1250 INTERSECTION LAYOUT
1"=10'

- GRADING NOTES**
1. THE EXISTING CONTOURS AND SURFACE ELEVATIONS INDICATED ON THESE PLANS HAVE BEEN LOCATED FROM REFERENCE INFORMATION SUPPLIED BY OTHERS AND SURFACE LOCATIONS SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY, BOTH HORIZONTALLY AND VERTICALLY, THE LOCATIONS OF ALL EXISTING HARD SURFACE PRIOR TO CONSTRUCTION, AND TO NOTIFY THE ENGINEER PROMPTLY OF ALL CONFLICTS OF THE WORK WITH EXISTING SURFACE ELEVATIONS.
 2. ALL OF THE ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 AND ALL HORIZONTAL POSITIONS ARE BASED ON THE NORTH AMERICAN DATUM OF 1983.
 3. MAXIMUM 2% SLOPE IN HANDICAP ACCESSIBLE AREAS IN ALL DIRECTIONS.
 4. MAXIMUM 2% CROSS SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
 5. MAXIMUM 5% LONGITUDINAL SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
 6. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES, NEW OR EXISTING, PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES WITH EXISTING OR NEW CONDITIONS.

- KEY NOTES**
- ① MATCH EXISTING
 - ② EARTHEN SWALE
 - ③ GRADE TO DRAIN
 - ④ BEGIN SWALE
 - ⑤ CURB AND GUTTER - SEE DETAIL 205/SHEET 26
 - ⑥ DIAGONAL PEDESTRIAN CURB RAMP - SEE DETAILS 226 & 233/SHEET 26
 - ⑦ DIRECTION PEDESTRIAN CURB RAMP WITHIN RADIUS - SEE DETAILS 231 & 233/SHEET 26

- LEGEND**
- EXISTING CONTOURS (0.5' INTERVAL)
 - NEW CONTOURS (0.5' INTERVAL)
 - DIRECTION OF FLOW
 - - - SWALE
 - CURB & GUTTER
 - NEW SPOT ELEVATION
 - fg = finish grade
 - g = gutter
 - fl = flowline
 - t = topsoil
 - sw = sidewalk
 - tc = top of curb



Parkhill



Parkhill.com

Cholla Road and County Road 1250
Paving Improvements

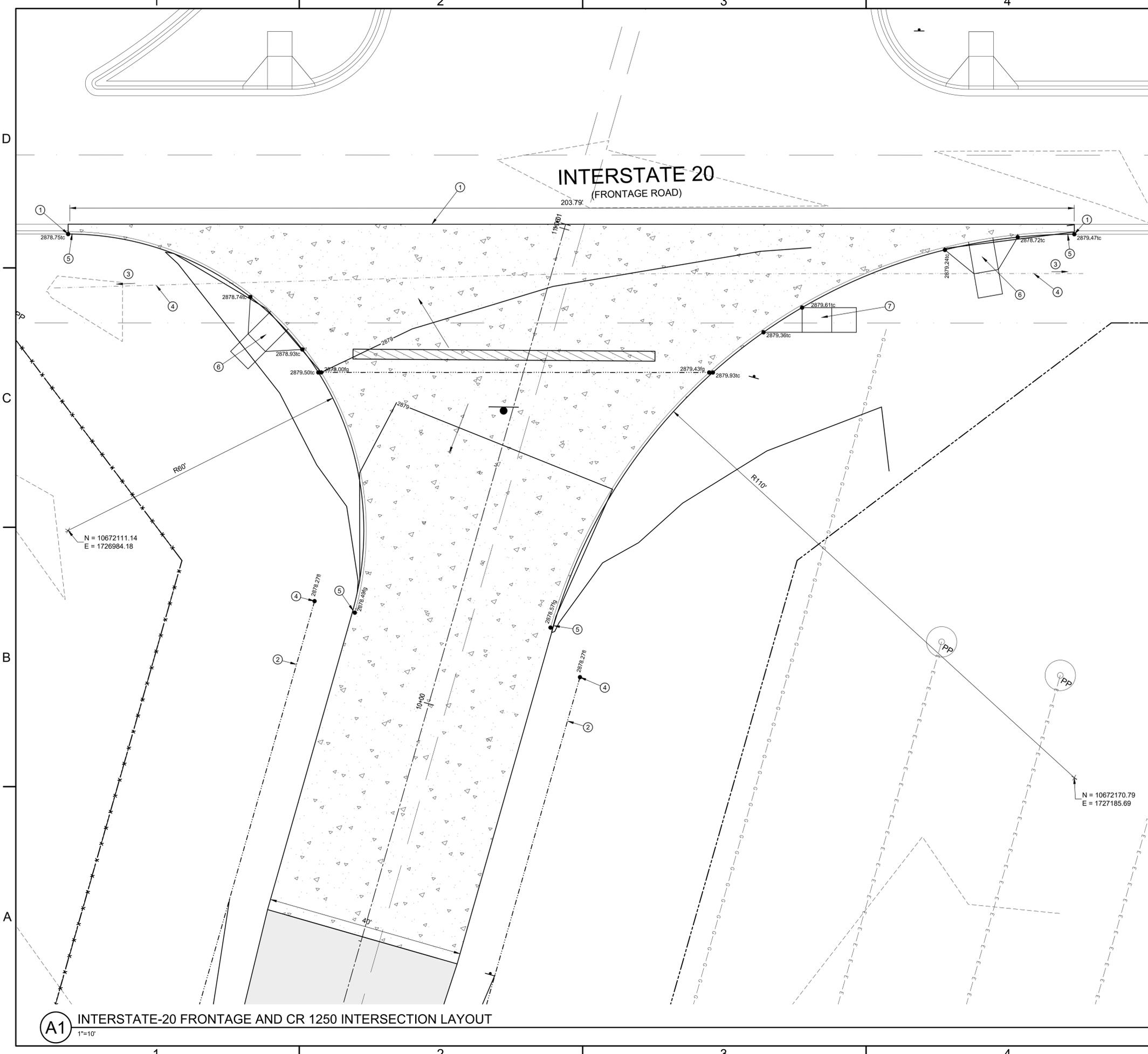
CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Cholla Road And CR 1250 Intersection Layout

FILE NAME: \\Data1\Projects\3\2020\6000.20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\BLOWUPS-6000.dwg LAYOUT NAME: 15 CR 1250 and I-20 Frontage Intersection Layout PRINTED: Friday, April 16, 2021 - 4:49pm USER: K.Christopher



GRADING NOTES

1. THE EXISTING CONTOURS AND SURFACE ELEVATIONS INDICATED ON THESE PLANS HAVE BEEN LOCATED FROM REFERENCE INFORMATION SUPPLIED BY OTHERS AND SURFACE LOCATIONS SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY, BOTH HORIZONTALLY AND VERTICALLY, THE LOCATIONS OF ALL EXISTING HARD SURFACE PRIOR TO CONSTRUCTION, AND TO NOTIFY THE ENGINEER PROMPTLY OF ALL CONFLICTS OF THE WORK WITH EXISTING SURFACE ELEVATIONS.
2. ALL OF THE ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 AND ALL HORIZONTAL POSITIONS ARE BASED ON THE NORTH AMERICAN DATUM OF 1983.
3. MAXIMUM 2% SLOPE IN HANDICAP ACCESSIBLE AREAS IN ALL DIRECTIONS.
4. MAXIMUM 2% CROSS SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
5. MAXIMUM 5% LONGITUDINAL SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
6. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES, NEW OR EXISTING, PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES WITH EXISTING OR NEW CONDITIONS.
7. COORDINATE ALL CONSTRUCTION AT THE INTERSECTION OF I-20 AND CR 1250 WITH TXDOT AND SET UP TRAFFIC CONTROL PER SHEETS 31-33.
TXDOT AREA ENGINEER MIDLAND:
FRED HERRERA (432) 694-2195
TXDOT MAINTENANCE SUPERVISOR MIDLAND:
JOHN CARRASCO (432) 694-7951



KEY NOTES

- 1 MATCH EXISTING
- 2 EARTHEN SWALE
- 3 GRADE TO DRAIN
- 4 BEGIN SWALE
- 5 CURB AND GUTTER - SEE DETAIL 205/SHEET 26
- 6 DIAGONAL PEDESTRIAN CURB RAMP - SEE DETAILS 226 & 233/SHEET 26
- 7 DIRECTION PEDESTRIAN CURB RAMP WITHIN RADIUS - SEE DETAILS 231 & 233/SHEET 26

LEGEND

- EXISTING CONTOURS (0.5' INTERVAL)
- NEW CONTOURS (0.5' INTERVAL)
- DIRECTION OF FLOW
- SWALE
- CURB & GUTTER
- NEW SPOT ELEVATION
- fg = finish grade
- g = gutter
- fl = flowline
- t = topsoil
- sw = sidewalk
- tc = top of curb

A1 INTERSTATE-20 FRONTAGE AND CR 1250 INTERSECTION LAYOUT
1"=10'

Parkhill



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Cholla Road and County Road 1250
Paving Improvements

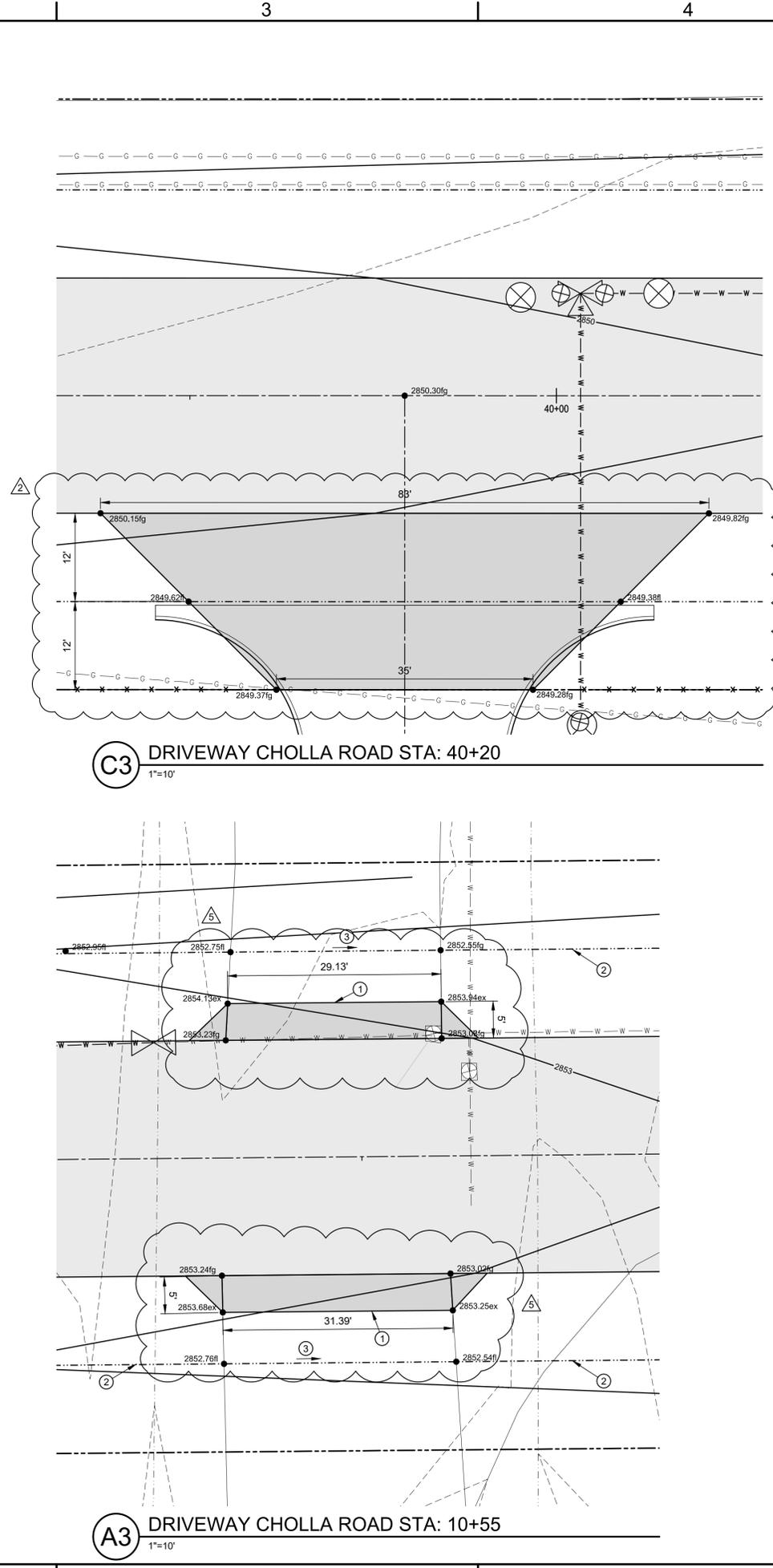
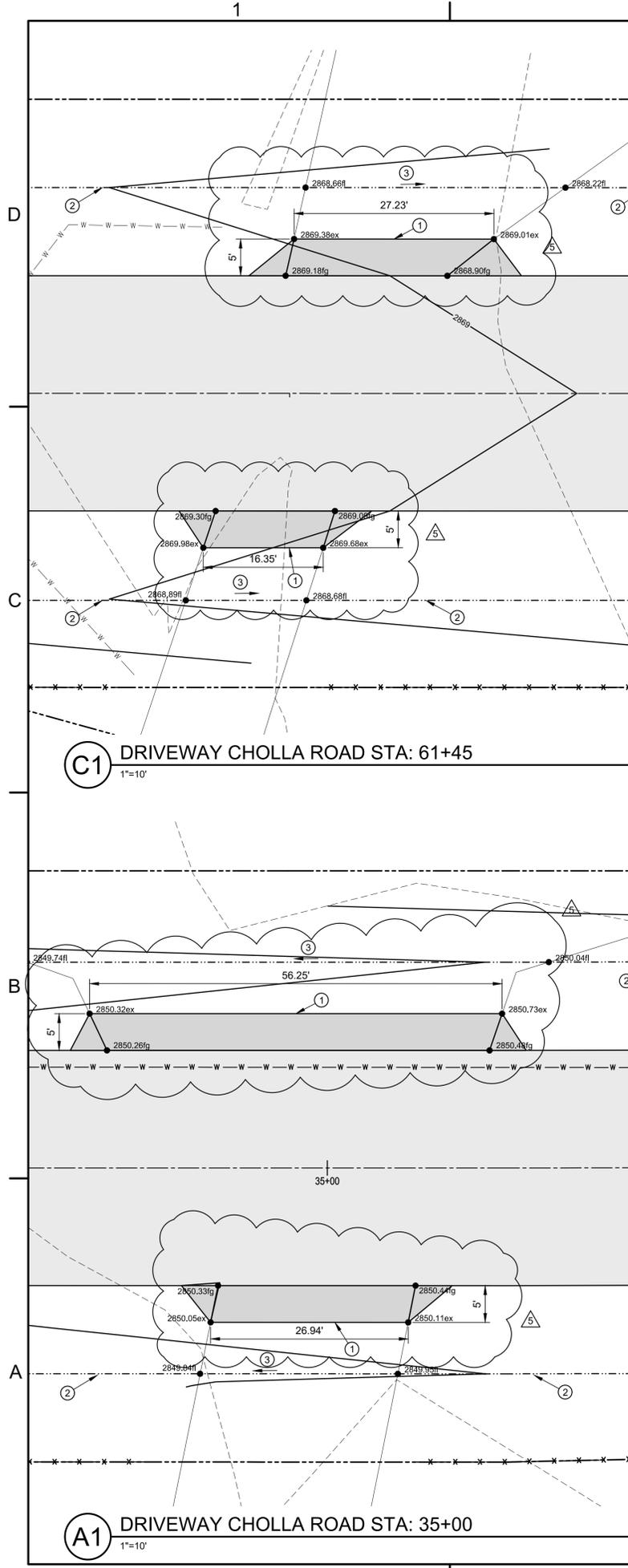
CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

CR 1250 And I-20
Frontage
Intersection
Layout

FILE NAME: A:\2020\6000.20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\BLOWUPS-6000.dwg LAYOUT NAME: 16 Driveway Layouts PRINTED: Friday, June 11, 2021 - 3:28am USER: BSanchez



- GRADING NOTES**
1. THE EXISTING CONTOURS AND SURFACE ELEVATIONS INDICATED ON THESE PLANS HAVE BEEN LOCATED FROM REFERENCE INFORMATION SUPPLIED BY OTHERS AND SURFACE LOCATIONS SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY, BOTH HORIZONTALLY AND VERTICALLY, THE LOCATIONS OF ALL EXISTING HARD SURFACE PRIOR TO CONSTRUCTION, AND TO NOTIFY THE ENGINEER PROMPTLY OF ALL CONFLICTS OF THE WORK WITH EXISTING SURFACE ELEVATIONS.
 2. ALL OF THE ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 AND ALL HORIZONTAL POSITIONS ARE BASED ON THE NORTH AMERICAN DATUM OF 1983.
 3. MAXIMUM 2% SLOPE IN HANDICAP ACCESSIBLE AREAS IN ALL DIRECTIONS.
 4. MAXIMUM 2% CROSS SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
 5. MAXIMUM 5% LONGITUDINAL SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
 6. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES, NEW OR EXISTING, PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES WITH EXISTING OR NEW CONDITIONS.



- KEY NOTES**
- 1 MATCH EXISTING
 - 2 EARTHEN SWALE
 - 3 GRADE TO DRAIN
 - 4 BEGIN SWALE
 - 5 CURB AND GUTTER - SEE DETAIL 205/SHEET 26
 - 6 DIAGONAL PEDESTRIAN CURB RAMP - SEE DETAILS 226 & 233/SHEET 26
 - 7 DIRECTION PEDESTRIAN CURB RAMP WITHIN RADIUS - SEE DETAILS 231 & 233/SHEET 26

- LEGEND**
- EXISTING CONTOURS (0.5' INTERVAL)
 - NEW CONTOURS (0.5' INTERVAL)
 - DIRECTION OF FLOW
 - SWALE
 - CURB & GUTTER
 - NEW SPOT ELEVATION
 - fg = finish grade
 - g = gutter
 - fl = flowline
 - t = topsoil
 - sw = sidewalk
 - tc = top of curb



Parkhill



Parkhill.com

Cholla Road and County Road 1250
Paving Improvements

CLIENT
Midland County

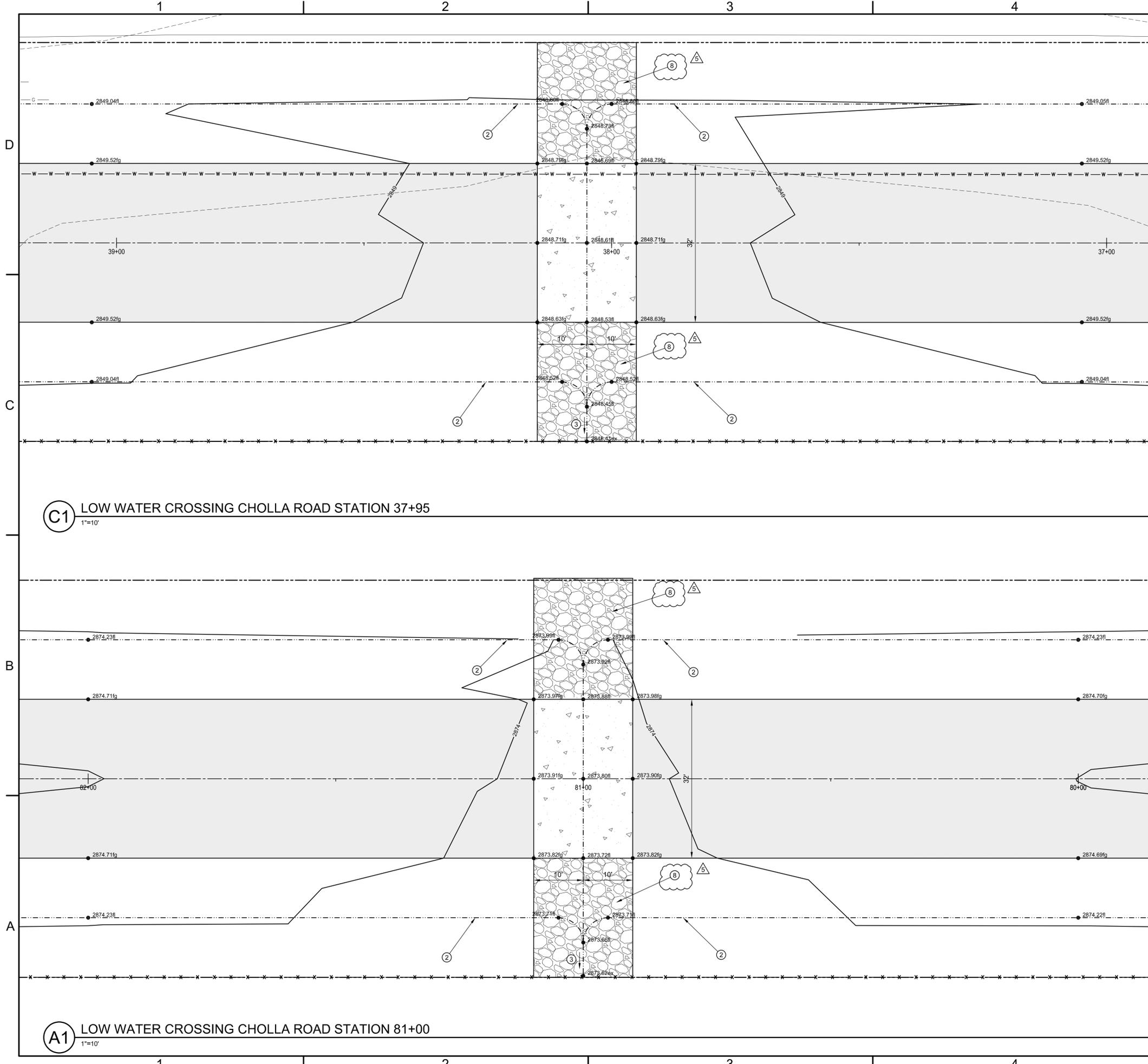
PROJECT NO.
6000.20

#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Driveway Layouts

16

FILE NAME: I:\Data\1\Projects\2020\6000_20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\BLOWUPS-6000.dwg LAYOUT NAME: 17 Low Water Crossing Layouts PRINTED: Thursday, June 10, 2021 - 3:03pm USER: K\Ward



GRADING NOTES

1. THE EXISTING CONTOURS AND SURFACE ELEVATIONS INDICATED ON THESE PLANS HAVE BEEN LOCATED FROM REFERENCE INFORMATION SUPPLIED BY OTHERS AND SURFACE LOCATIONS SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY, BOTH HORIZONTALLY AND VERTICALLY, THE LOCATIONS OF ALL EXISTING HARD SURFACE PRIOR TO CONSTRUCTION, AND TO NOTIFY THE ENGINEER PROMPTLY OF ALL CONFLICTS OF THE WORK WITH EXISTING SURFACE ELEVATIONS.
2. ALL OF THE ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 AND ALL HORIZONTAL POSITIONS ARE BASED ON THE NORTH AMERICAN DATUM OF 1983.
3. MAXIMUM 2% SLOPE IN HANDICAP ACCESSIBLE AREAS IN ALL DIRECTIONS.
4. MAXIMUM 2% CROSS SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
5. MAXIMUM 5% LONGITUDINAL SLOPE FOR HANDICAP ACCESSIBLE SIDEWALKS.
6. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND GRADES, NEW OR EXISTING, PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES WITH EXISTING OR NEW CONDITIONS.



KEY NOTES

- ① MATCH EXISTING
- ② EARTHEN SWALE
- ③ GRADE TO DRAIN
- ④ BEGIN SWALE
- ⑤ CURB AND GUTTER - SEE DETAIL 205/SHEET 26
- ⑥ DIAGONAL PEDESTRIAN CURB RAMP - SEE DETAILS 226 & 233/SHEET 26
- ⑦ DIRECTION PEDESTRIAN CURB RAMP WITHIN RADIUS - SEE DETAILS 231 & 233/SHEET 26
- ⑧ STONE RIPRAP - SEE FIGURE 4/SHEET 35

LEGEND

- EXISTING CONTOURS (0.5' INTERVAL)
- NEW CONTOURS (0.5' INTERVAL)
- DIRECTION OF FLOW
- SWALE
- CURB & GUTTER
- NEW SPOT ELEVATION
- fg = finish grade
- g = gutter
- fl = flowline
- t = topsoil
- sw = sidewalk
- tc = top of curb



Parkhill



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Cholla Road and County Road 1250
Paving Improvements

CLIENT
Midland County

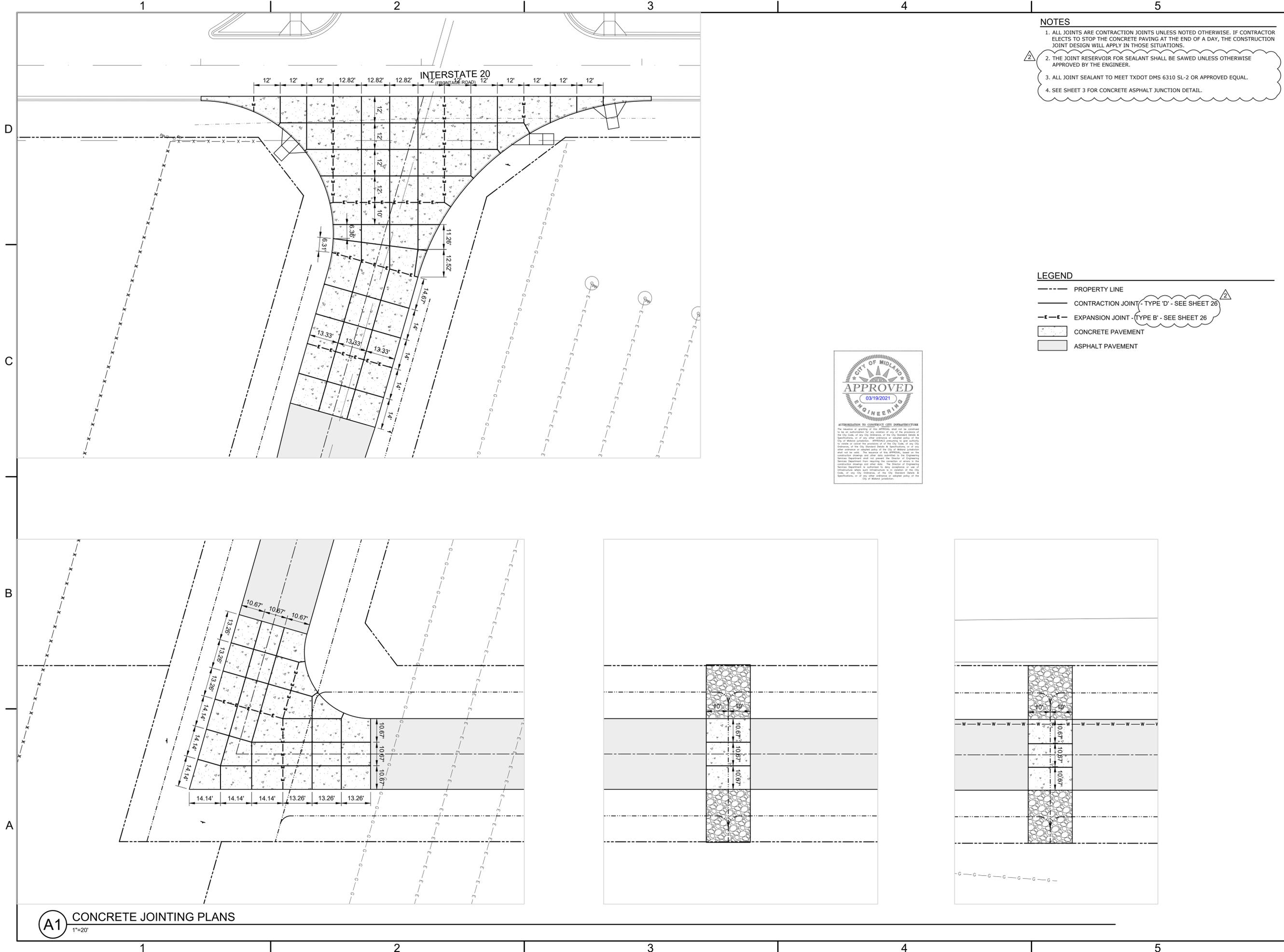
PROJECT NO.
6000.20

#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Low Water Crossing Layouts

17

FILE NAME: A:\2020\6000_2003_DSGN01_DWG\050_CIVIL\00_SHEETS\JOINT-6000.dwg LAYOUT NAME: 18 Concrete Jointing Plan PRINTED: Wednesday, March 03, 2021 - 11:52pm USER: KChristopher



- NOTES**
1. ALL JOINTS ARE CONTRACTION JOINTS UNLESS NOTED OTHERWISE. IF CONTRACTOR ELECTS TO STOP THE CONCRETE PAVING AT THE END OF A DAY, THE CONSTRUCTION JOINT DESIGN WILL APPLY IN THOSE SITUATIONS.
 2. THE JOINT RESERVOIR FOR SEALANT SHALL BE SAWED UNLESS OTHERWISE APPROVED BY THE ENGINEER.
 3. ALL JOINT SEALANT TO MEET TXDOT DMS 6310 SL-2 OR APPROVED EQUAL.
 4. SEE SHEET 3 FOR CONCRETE ASPHALT JUNCTION DETAIL.

- LEGEND**
- PROPERTY LINE
 - CONTRACTION JOINT - TYPE 'D' - SEE SHEET 26
 - E-E- EXPANSION JOINT - TYPE 'B' - SEE SHEET 26
 - [Pattern] CONCRETE PAVEMENT
 - [Pattern] ASPHALT PAVEMENT



ATTENTION TO CONCRETE CITY INFRASTRUCTURE
 The location or spacing of this structure, and all its components, shall be approved by the City Council of the City of Midland, Texas, or its authorized representative, prior to construction. APPROVED (pending to give authority to locate or space the structure) of the City Council of any City or other authority or approved joint of the City of Midland, Texas, and will be void. The location of this structure, based on the construction drawings and other data submitted to the Engineering Service Department shall not exceed the Director of Engineering Service Department's approval. The Director of Engineering Service Department is authorized to, in any condition or use, all structures which such structures is in violation of the City of Midland, Texas, or any other authority or approved joint of the City of Midland, Texas.

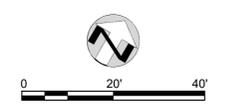
A1 CONCRETE JOINTING PLANS
 1"=20'

Parkhill



Parkhill.com

Cholla Road and County Road 1250
 Paving Improvements



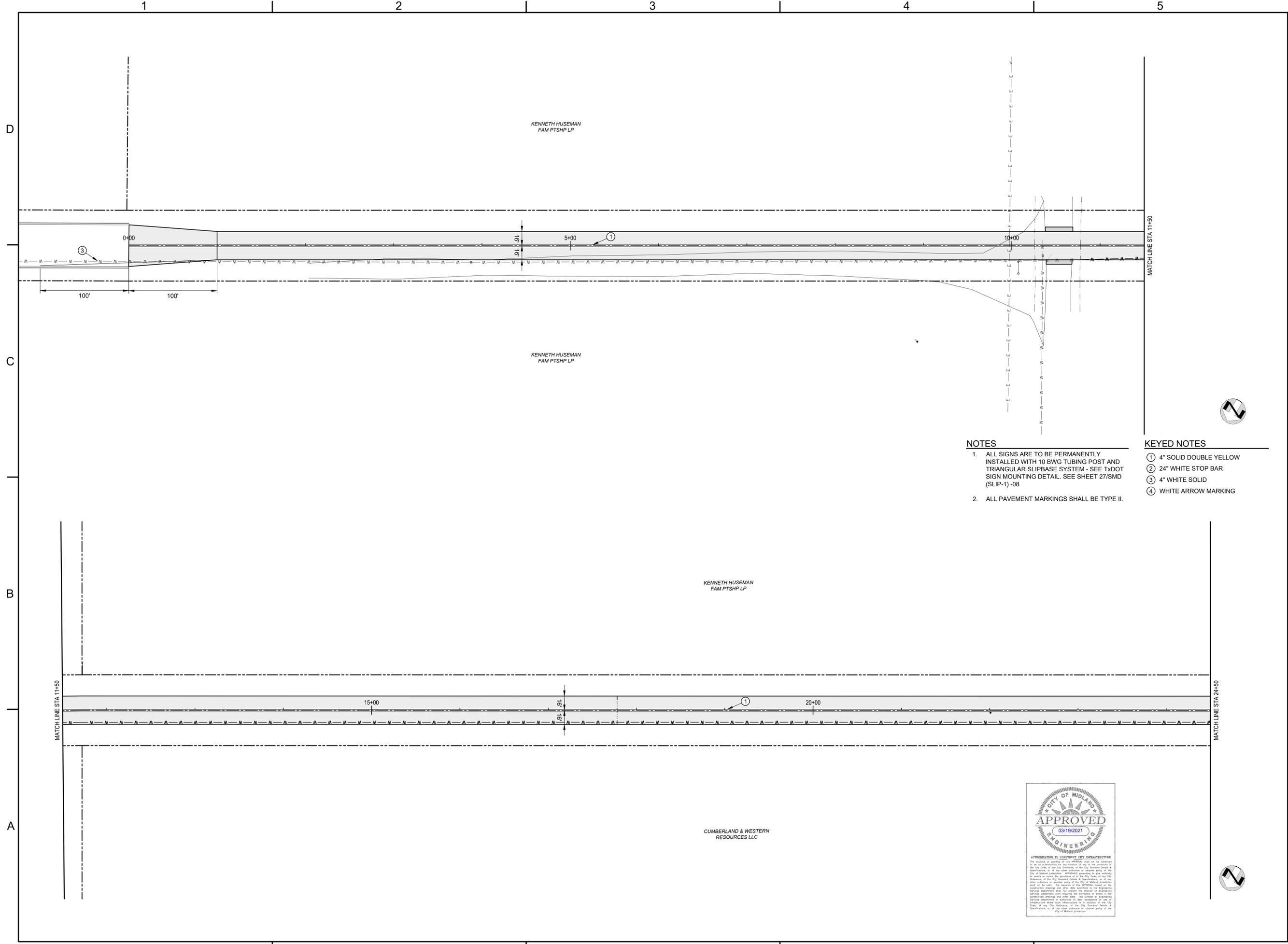
CLIENT
 Midland County

PROJECT NO.
 6000.20

#	DATE	DESCRIPTION
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Concrete Jointing Plan
18

FILE NAME: A:\2020\6000_20\03_DSGN\01_DWG\050_CIVIL\00_SHEET\STRAFF_01+6000.dwg LAYOUT NAME: 19 Sign and Pavement Marking Plan PRINTED: Wednesday, March 03, 2021 - 11:53pm USER: KChristopher



NOTES

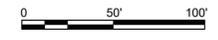
1. ALL SIGNS ARE TO BE PERMANENTLY INSTALLED WITH 10 BWG TUBING POST AND TRIANGULAR SLIPBASE SYSTEM - SEE TxDOT SIGN MOUNTING DETAIL. SEE SHEET 27/SMD (SLIP-1) -08
2. ALL PAVEMENT MARKINGS SHALL BE TYPE II.

KEYED NOTES

- ① 4" SOLID DOUBLE YELLOW
- ② 24" WHITE STOP BAR
- ③ 4" WHITE SOLID
- ④ WHITE ARROW MARKING



Cholla Road and County Road 1250
Paving Improvements



CLIENT
Midland County

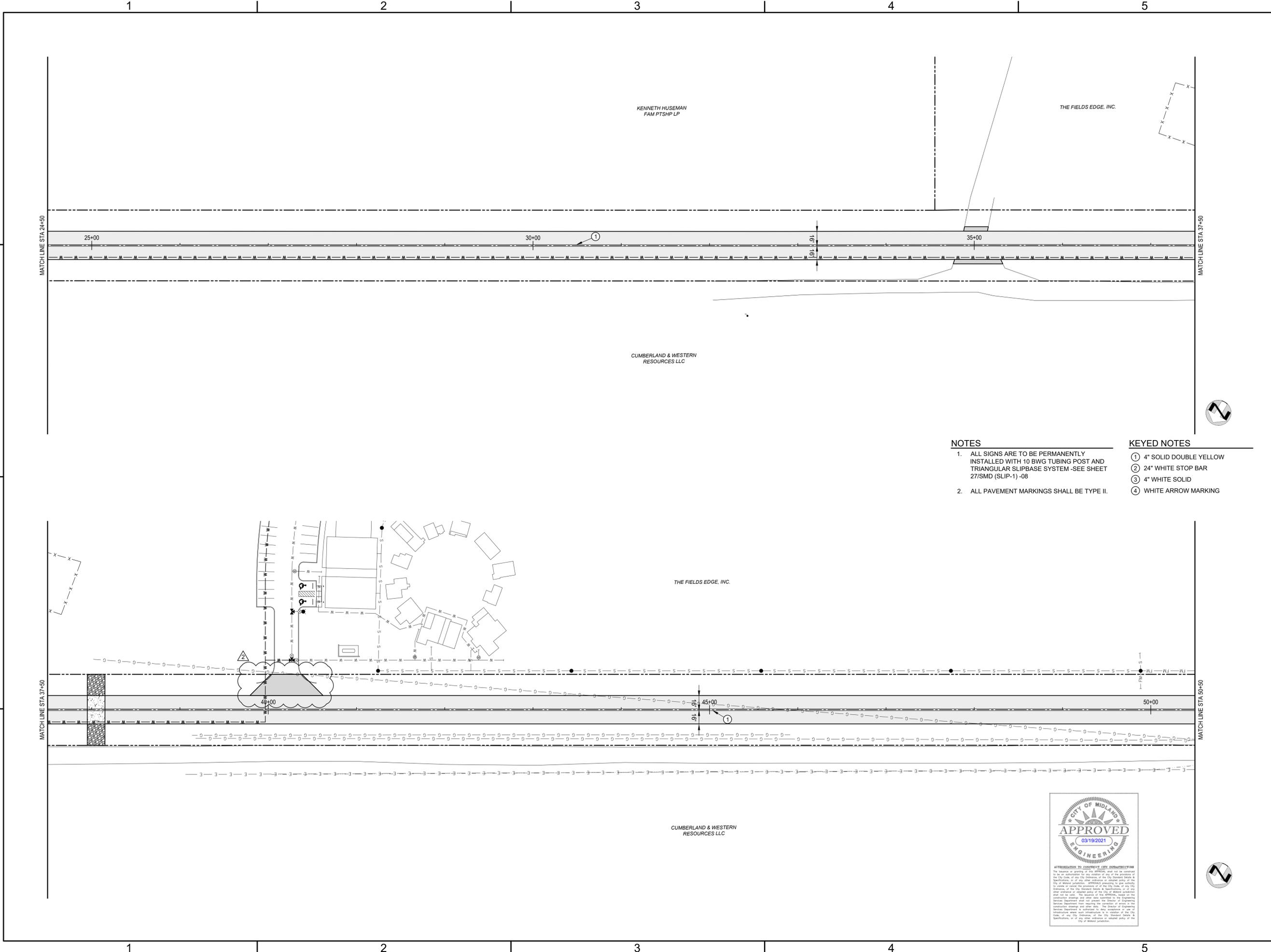
PROJECT NO.
6000.20

#	DATE	DESCRIPTION
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection



Sign And Pavement Marking Plan

FILE NAME: A:\2020\6000_20\03_DSGN\01_DWG\050_CIVIL\00_SHEET\STRAFF_01+6000.dwg LAYOUT NAME: 20 Sign and Pavement Marking Plan PRINTED: Wednesday, March 03, 2021 - 11:53pm USER: KChristopher



NOTES

1. ALL SIGNS ARE TO BE PERMANENTLY INSTALLED WITH 10 BWG TUBING POST AND TRIANGULAR SLIPBASE SYSTEM - SEE SHEET 27/SMD (SLIP-1) -08
2. ALL PAVEMENT MARKINGS SHALL BE TYPE II.

KEYED NOTES

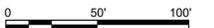
- ① 4" SOLID DOUBLE YELLOW
- ② 24" WHITE STOP BAR
- ③ 4" WHITE SOLID
- ④ WHITE ARROW MARKING

Parkhill



Parkhill.com

Cholla Road and County Road 1250
Paving Improvements



CLIENT
Midland County

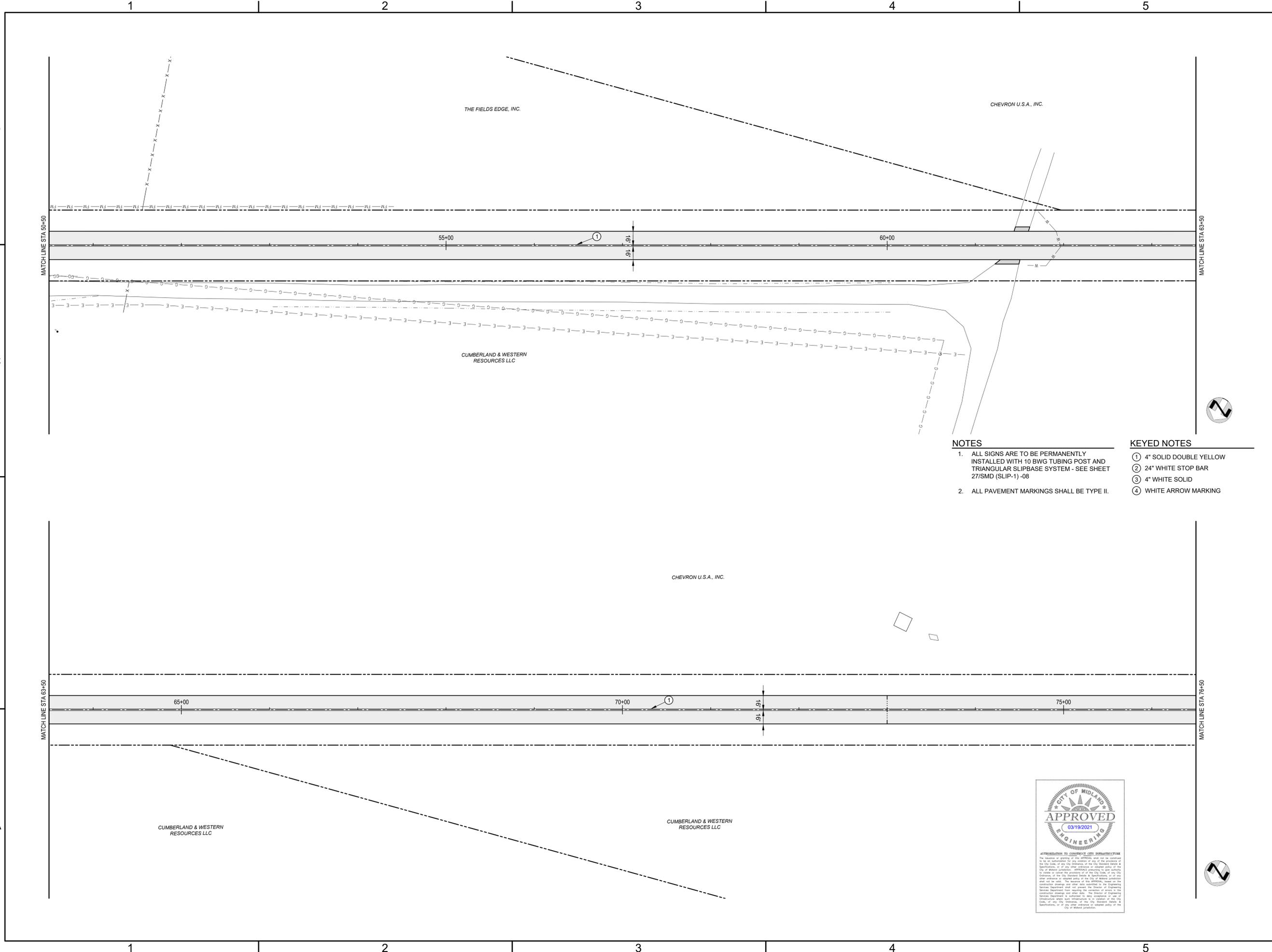
PROJECT NO.
6000.20

#	DATE	DESCRIPTION
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Sign And Pavement Marking Plan

20





NOTES

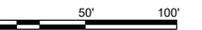
1. ALL SIGNS ARE TO BE PERMANENTLY INSTALLED WITH 10 BWG TUBING POST AND TRIANGULAR SLIPBASE SYSTEM - SEE SHEET 27/SMD (SLIP-1) -08
2. ALL PAVEMENT MARKINGS SHALL BE TYPE II.

KEYED NOTES

- ① 4" SOLID DOUBLE YELLOW
- ② 24" WHITE STOP BAR
- ③ 4" WHITE SOLID
- ④ WHITE ARROW MARKING



Cholla Road and County Road 1250
Paving Improvements



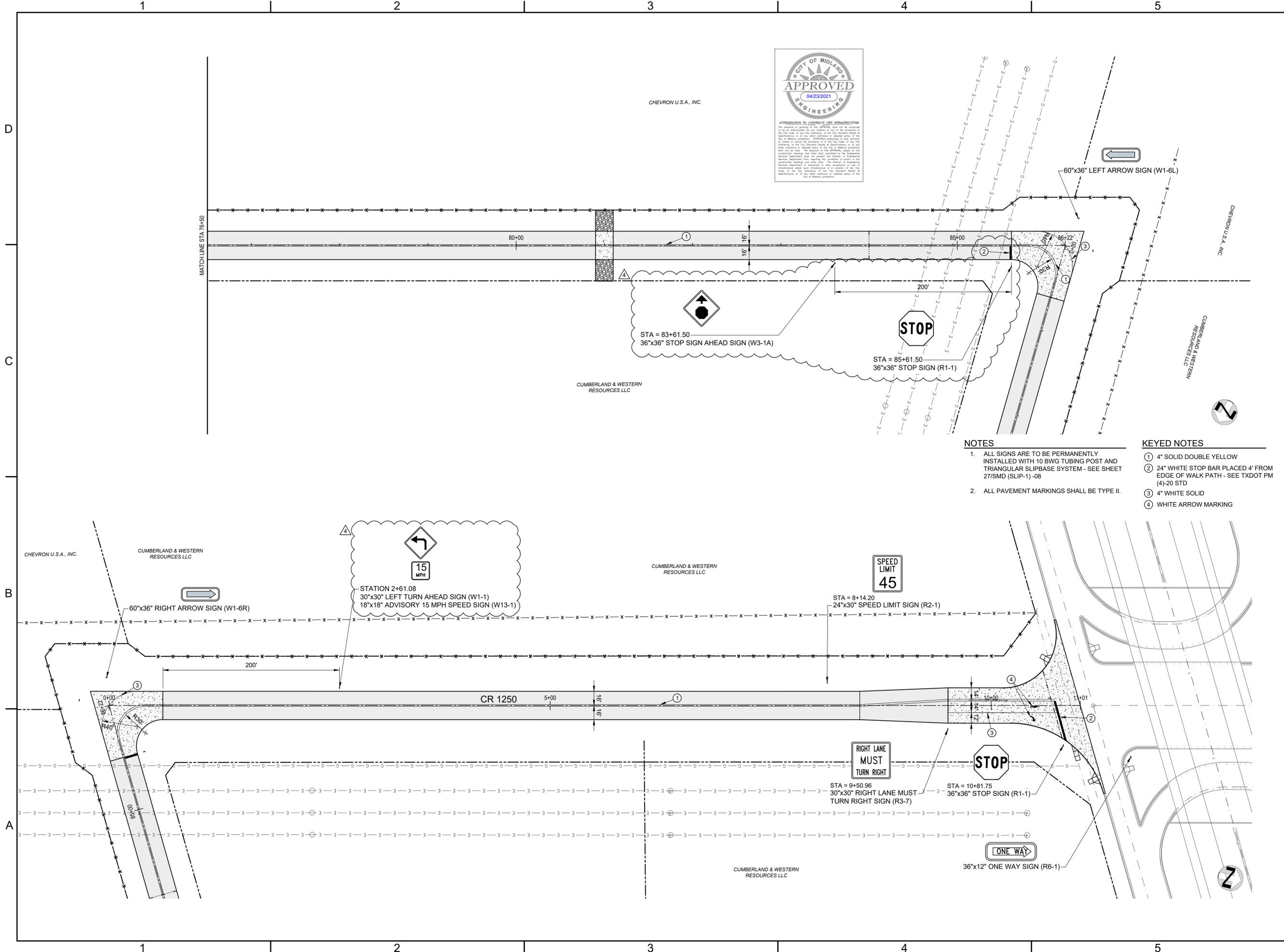
CLIENT
Midland County

PROJECT NO.
6000.20

#	DATE	DESCRIPTION
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Sign And Pavement Marking Plan

FILE NAME: \\Data1\Projects\3\2020\6000.20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\TRAF_01-6000.dwg LAYOUT NAME: 22 Sign and Pavement Marking Plan PRINTED: Friday, April 16, 2021 - 4:51pm USER: KChristopher



NOTES

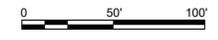
1. ALL SIGNS ARE TO BE PERMANENTLY INSTALLED WITH 10 BWG TUBING POST AND TRIANGULAR SLIPBASE SYSTEM - SEE SHEET 27/SMD (SLIP-1) -08
2. ALL PAVEMENT MARKINGS SHALL BE TYPE II.

KEYED NOTES

- ① 4" SOLID DOUBLE YELLOW
- ② 24" WHITE STOP BAR PLACED 4' FROM EDGE OF WALK PATH - SEE TXDOT PM (4)-20 STD
- ③ 4" WHITE SOLID
- ④ WHITE ARROW MARKING

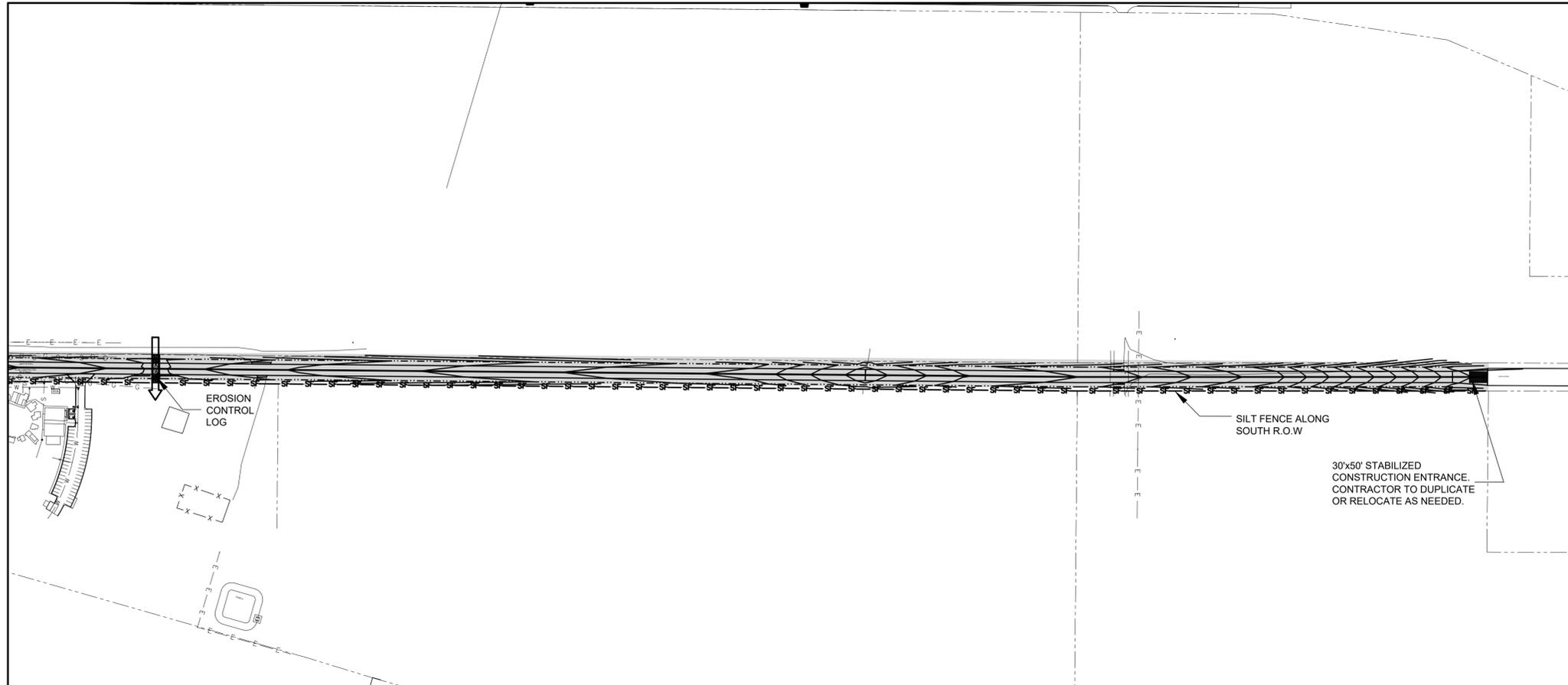


Cholla Road and County Road 1250
Paving Improvements

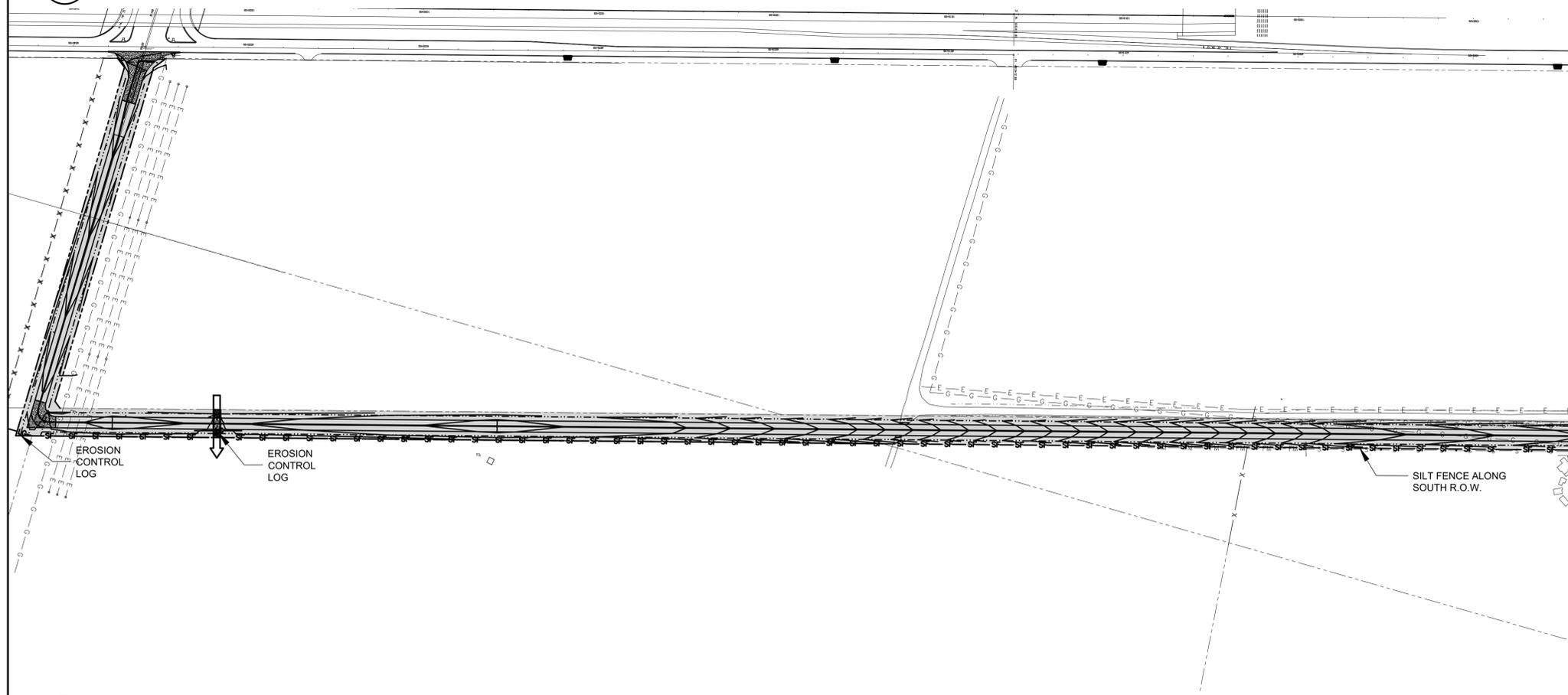


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Midland County	
PROJECT NO.	
6000.20	
4	04/16/2021 Midland County Comments
3	03/17/2021 Response to 2nd Comments
2	03/03/2021 Response to Comments
1	02/19/2021 Waterline Projection
#	DATE DESCRIPTION

FILE NAME: A:\2020\6000.20\03_DSGN\01_DWG\050_CIVIL\00_SHEETS\SWPPP_01-6000.dwg LAYOUT NAME: 23 Erosion Control Plan PRINTED: Wednesday, March 17, 2021 - 10:58am USER: KChristopher



C1 STORMWATER POLLUTION PREVENTION PLAN - PHASE 1
1"=200'



A1 STORMWATER POLLUTION PREVENTION PLAN - PHASE 2
1"=200'

NOTE

- A. THIS MAP IS PART OF A TPDES STORMWATER POLLUTION PREVENTION PLAN. REFER TO THE TEXT PORTION OF THE PLAN FOR ADDITIONAL INFORMATION, REQUIREMENTS AND PROCEDURES. PLAN TO BE DEVELOPED BY THE CONTRACTOR.
- B. REFER TO SHEET 24 FOR EROSION CONTROL DETAILS
- C. REMOVE AND REPLACE CONCRETE WASHOUT WHEN 75% OF THE VOLUME IS REACHED.
- D. CONCRETE WASHOUT AREA MAY BE RELOCATED OR DUPLICATED AS NECESSARY. PORTABLE WASHOUT IS ALLOWED.
- E. THE USE OF A PORTABLE CONCRETE WASHOUT STATION IS ACCEPTABLE.
- F. SUBMIT ALTERNATE BMP'S FOR APPROVAL PRIOR TO INSTALLATION.
- G. PER THE CITY STORM WATER ADMINISTRATORS: STABILIZE BORROWED STOCKPILES AND STAGING AREAS AT THE COMPLETION OF THE PROJECT
- H. PRIOR TO ANY SOIL GRADING ACTIVITIES, CONTRACTOR TO CALL 432-685-2365 FOR INSPECTION.
- I. **PRE-APPROVED DEVELOPMENT SWPPP REQUIREMENTS:**

- I.A. **"DEVELOPERS ARE NOW REQUIRED TO CALL 432-685-7517 TO REQUEST A STORMWATER INSPECTION BEFORE GRADING LAND FOR PROJECT SITES"**
- I.B. **FOR COMMERCIAL PLANS THAT ARE IN THE PRE-APPROVAL STAGES, THE DEVELOPER, CONTRACTOR, BUILDER ETC. MUST HAVE THE FOLLOWING STORMWATER MEASURES IN PLACE, PER THE APPROVED CITY STANDARD, PRIOR TO BEGINNING ANY CONSTRUCTION. THESE MEASURES INCLUDE:**
 - I.B.A. **SILT FENCE - INSTALL PER CITY STANDARDS AND MUST BE TRENCHED 6 INCHES INTO THE GROUND WITH METAL MESH FACING THE STREET AND J-HOOKED AND BACKFILLED. ADDITIONALLY, FENCE MUST BE STAKED EVERY 6 FT. TO ENSURE PROPER STABILITY.**
 - I.B.B. **CONSTRUCTION SITE ENTRANCE - CITY STANDARDS CALL FOR "BULL ROCK" WITH A 6-8-INCH DIAMETER ROCK. THE TYPICAL ENTRANCE MUST BE 50 FT. LONG AND 14 FT. WIDE. (LENGTH AND WIDTH CAN BE ADJUSTED DEPENDING ON SITE SIZE)**
 - I.B.C. **INLET PROTECTION - IF THE SITE IS BORDERED BY STORMWATER INLETS, THEY MUST BE PROTECTED BY THE DEVELOPER USING SANDBAGS OR FIBER WATTLES.**
 - I.B.D. **TCEQ REQUIRES ALL DEVELOPERS IN THE CITY OF MIDLAND THAT DEVELOP SMALL SITES (1-5 ACRES CONSTRUCTION SITE); AND LARGE SITES (5 OR MORE ACRES CONSTRUCTION SITE) TO SUBMIT A SIGNED COPY OF THE NOTICE OF INTENT (NOI) OR CONSTRUCTION SITE NOTICE (CSN) TO THE CITY OF MIDLAND AT LEAST 7 DAYS PRIOR TO THE BEGINNING ANY SITE PREPARATION.**
- I.C. **IF THERE ARE ANY QUESTIONS OR CONCERNS, PLEASE CONTACT THE CITY OF MIDLAND, STORMWATER DIVISION AT 432-685-7517.**

FINAL SITE STABILIZATION: IN ACCORDANCE WITH PARTS III.G.1 AND III.G.2 OF THE CONSTRUCTION STORMWATER GENERAL PERMIT, STABILIZATION OF ALL DISTURBED AREAS MUST, AT A MINIMUM, BE INITIATED IMMEDIATELY (I.E. AS SOON AS PRACTICABLE, BUT NO LATER THAN THE END OF THE NEXT WORK DAY) WHENEVER ANY EARTHWORK ACTIVITIES HAVE PERMANENTLY CEASED ON ANY PORTION OF THE SITE. THE CITY WILL NOT SUPPORT A C.O. UNLESS THIS REQUIREMENT HAS BEEN FULFILLED.

CONTRACTOR BEWARE: THIS REQUIREMENT WILL BE ENFORCED WITH ZEAL



LEGEND

- AREA AFFECTED BY PROJECT (APPROX. 17.9 ACRES)
- SF SILT FENCE
- RK ROCK FILTER DAM
- LC EROSION CONTROL LOG
- EXISTING CONTOURS (1' INTERVAL)
- PROPOSED DRAINAGE PATH WITH PERCENT SLOPE
- STABILIZED CONSTRUCTION ENTRANCE
- NEW CONTOURS (1' INTERVAL)
- FLOW DIRECTION (EXISTING)
- FLOW DIRECTION (PROPOSED)



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Midland County		
PROJECT NO.		
6000.20		
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3	03/17/2021	Response to 2nd Comments
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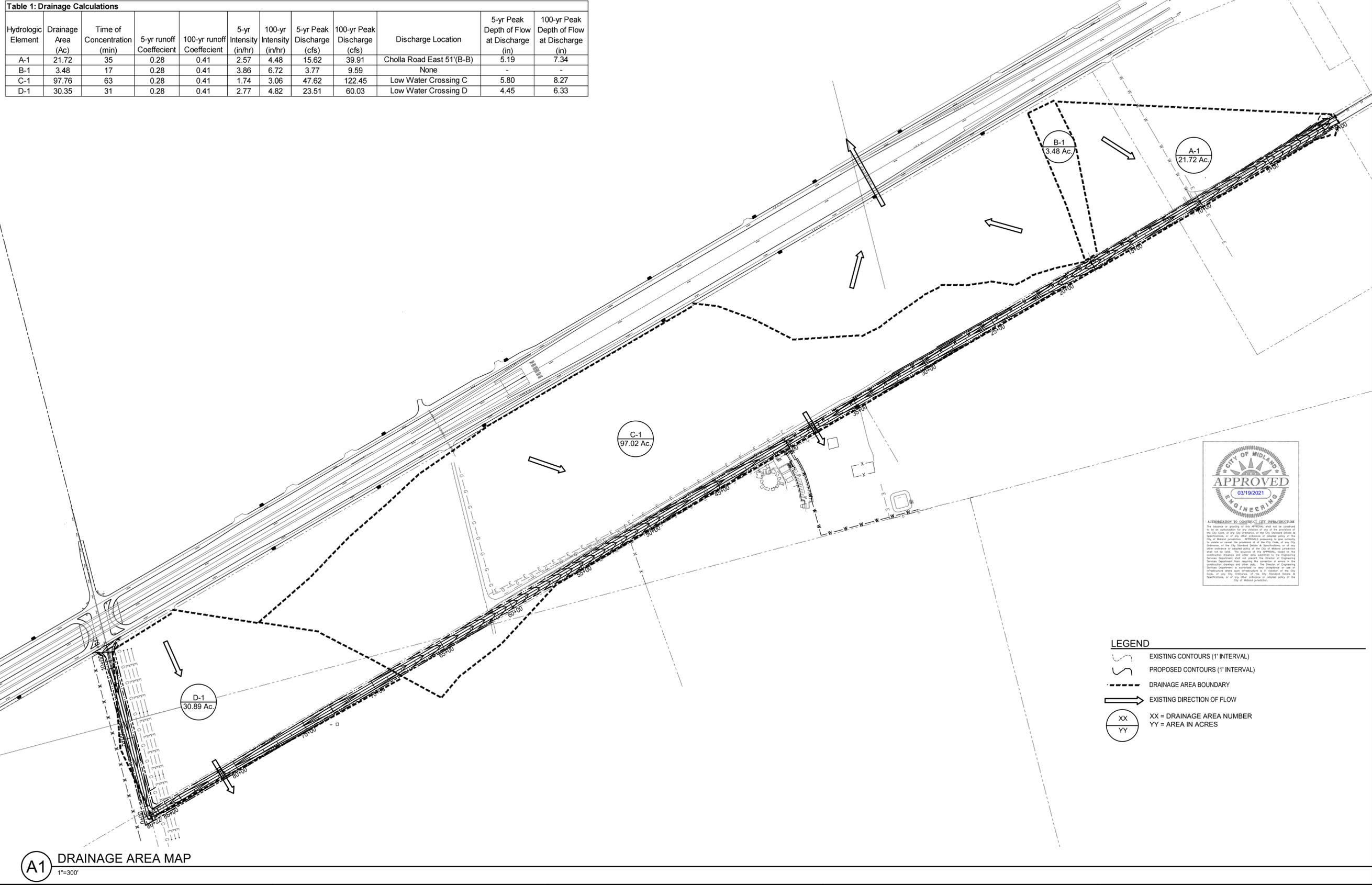
FILE NAME: A:\2020\6000_2003_DSGN01_DWG\050_CIVIL\00_SHEETS\SDAM_01-6000.dwg LAYOUT NAME: 25 Drainage Area Map and Drainage Calculations PRINTED: Wednesday, March 03, 2021 - 11:55pm USER: KChristopher

DRAINAGE NARRATIVE:
 THE DRAINAGE AREAS WERE DETERMINED BASED ON SURVEY DATA, USGS CONTOURS, AND LIDAR INFORMATION. THE RATIONAL METHOD WAS USED TO CALCULATE THE PEAK DISCHARGE FLOWRATES OF EACH AREA. MANNINGS EQUATION AND FLOW MASTER SOFTWARE WAS USED TO CALCULATE THE WATER DEPTH AT EACH OF THE DISCHARGE LOCATIONS. THE RESULTS ARE SHOWN IN TABLE 1.

DRAINAGE AREA B-1 DOES NOT HAVE A DISCHARGE POINT. IT WAS DETERMINED SINCE THE AREA AND PEAK FLOW RATES ARE MINIMAL THAT THIS AREA DOES NOT NEED TO DRAIN AND CAN ADEQUATELY STORE ITS RUNOFF WITHIN THE B-1 DRAINAGE AREA.

Table 1: Drainage Calculations

Hydrologic Element	Drainage Area (Ac)	Time of Concentration (min)	5-yr runoff Coefficient	100-yr runoff Coefficient	5-yr Intensity (in/hr)	100-yr Intensity (in/hr)	5-yr Peak Discharge (cfs)	100-yr Peak Discharge (cfs)	Discharge Location	5-yr Peak Depth of Flow at Discharge (in)	100-yr Peak Depth of Flow at Discharge (in)
A-1	21.72	35	0.28	0.41	2.57	4.48	15.62	39.91	Cholla Road East 51'(B-B)	5.19	7.34
B-1	3.48	17	0.28	0.41	3.86	6.72	3.77	9.59	None	-	-
C-1	97.76	63	0.28	0.41	1.74	3.06	47.62	122.45	Low Water Crossing C	5.80	8.27
D-1	30.35	31	0.28	0.41	2.77	4.82	23.51	60.03	Low Water Crossing D	4.45	6.33



- LEGEND**
- EXISTING CONTOURS (1' INTERVAL)
 - PROPOSED CONTOURS (1' INTERVAL)
 - DRAINAGE AREA BOUNDARY
 - EXISTING DIRECTION OF FLOW
 - XX = DRAINAGE AREA NUMBER
 - YY = AREA IN ACRES

A1 DRAINAGE AREA MAP
 1"=300'

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Cholla Road and County Road 1250
 Paving Improvements



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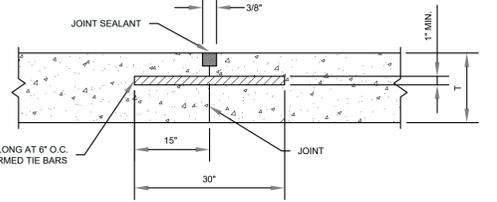
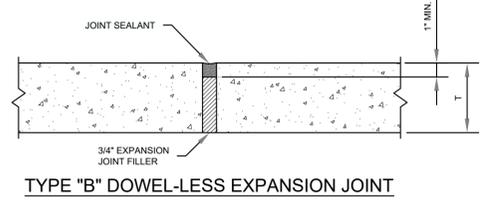
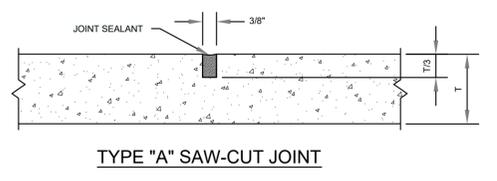
PROJECT NO.
 6000.20

#	DATE	DESCRIPTION
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

Drainage Area Map And Drainage Calculations

25

D

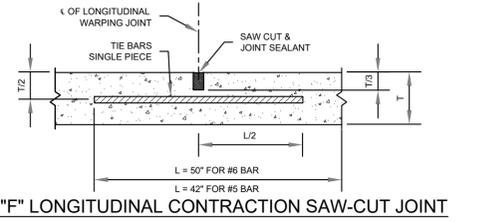
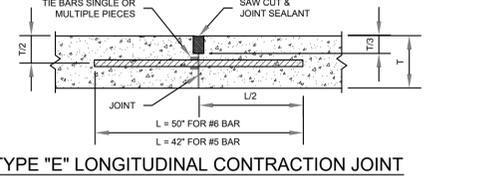
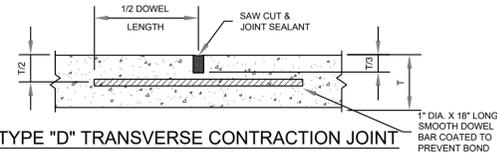


DRAWN: DPM	CHECKED: JCF	APPROVED: MCC
EFFECTIVE DATE: 10/01/2018	SCALE: NTS	DETAIL: 234(A)

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TYPICAL CONCRETE JOINTS

C



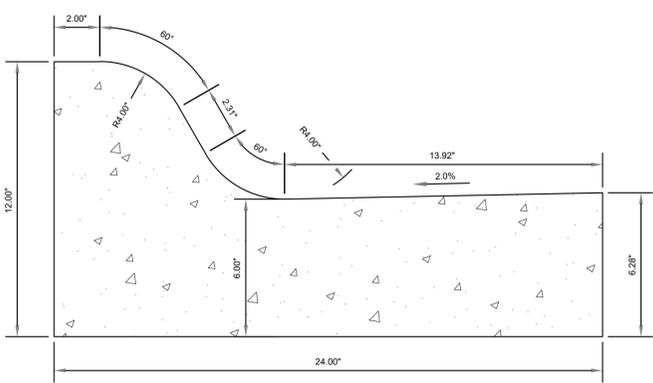
- NOTES:**
1. JOINTING, DOWEL AND TIE BAR DESIGN REQUIREMENTS ARE SUBJECT TO CHANGE FOR ARTERIAL OR HIGHER CLASSIFICATION ROADS IF DEEMED NECESSARY BY THE CITY ENGINEER.
 2. TYPICAL CONCRETE PAVEMENT JOINT SPACING IS TO BE NO LESS THAN 10' X 10' AND NO MORE THAN 15' X 15'.
 3. DOWELS ARE TO BE SPACED AT 12" INTERVALS WITH MINIMUM 18" SEPARATION FROM PARALLEL JOINTS OR EDGES OF PAVEMENT.
 4. TIE BARS ARE TO BE SET AT 24" INTERVALS WITH MINIMUM 18" SEPARATION FROM PARALLEL JOINTS OR EDGES OF PAVEMENT.
 5. ALL JOINT SEALANT TO BE TxDOT DMS-6310 SL-2 OR APPROVED EQUAL.
 6. MATERIALS AND CONSTRUCTION METHODS TO CONFORM TO CITY OF MIDLAND STANDARDS AND SPECIFICATIONS.
 7. CONSTRUCT AS SHOWN UNLESS OTHERWISE APPROVED IN WRITING BY THE CITY ENGINEER.

DRAWN: DPM	CHECKED: JCF	APPROVED: MCC
EFFECTIVE DATE: 10/01/2018	SCALE: NTS	DETAIL: 234(B)

MIDLAND Engineering Services

TYPICAL CONCRETE JOINTS

B



- NOTES:**
1. CONCRETE TO BE CITY OF MIDLAND CLASS "C", 3600 PSI. USE FIBER REINFORCED CONCRETE PAVEMENT THROUGHOUT.
 2. SEAL CONCRETE JOINT AT FACE OF CURB WHEN ADJACENT TO CONCRETE PAVEMENT AND NOT INTEGRAL WITH THAT PAVEMENT.
 3. MATERIALS AND CONSTRUCTION METHODS TO CONFORM TO CITY OF MIDLAND STANDARDS AND SPECIFICATIONS.
 4. CONSTRUCT AS SHOWN UNLESS OTHERWISE APPROVED IN WRITING BY THE CITY ENGINEER.

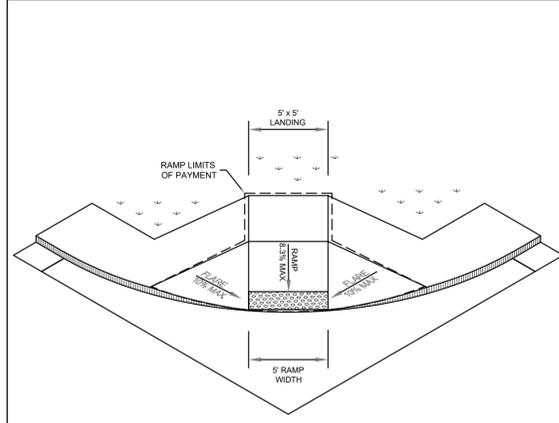
DRAWN: DPM	EFFECTIVE DATE: 10/01/2018	SCALE: NTS	DETAIL: 205
CHECKED: JCF			
APPROVED: MCC			

MIDLAND Engineering Services

TYPE A STANDARD CURB AND GUTTER



A

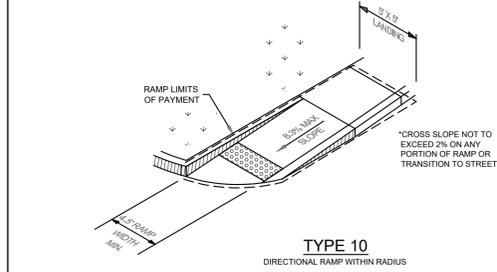


DRAWN: DPM	EFFECTIVE DATE: 10/01/2018	SCALE: NTS	DETAIL: 226
CHECKED: JCF			
APPROVED: MCC			

MIDLAND Engineering Services

DIAGONAL CURB RAMP

- CURB RAMP NOTES:**
1. ADJUST CURB RAMP LOCATION AND OR TYPE SO THAT NO OBSTRUCTION IS LOCATED WITHIN THE LANDING AREA.
 2. ALL SLOPES ARE MAXIMUM ALLOWABLE. THE LEAST POSSIBLE SLOPE THAT WILL STILL DRAIN PROPERLY SHOULD BE USED.
 3. LANDINGS SHALL BE 5' X 5' MINIMUM WITH A MAXIMUM 2% SLOPE IN ANY DIRECTION. WHERE OBSTRUCTIONS EXIST, THERE SHALL BE A 3' MINIMUM CLEARANCE IN WIDTH AND LENGTH OF THE LANDING.
 4. MANEUVERING SPACE AT THE BOTTOM OF CURB RAMP SHALL BE A MINIMUM OF 4' X 4' WHOLLY CONTAINED WITHIN THE CROSSWALK AND TO THE ENTIRE OUTSIDE OF THE PARALLEL VEHICULAR TRAVEL PATH.
 5. MAXIMUM ALLOWABLE CROSS SLOPE ON SIDEWALK AND CURB RAMP SURFACES IS 2%.
 6. CURB RAMP WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. EITHER BECAUSE THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR BECAUSE THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED. OTHERWISE, PROVIDE FLARED SIDES.
 7. ADDITIONAL INFORMATION ON CURB RAMP LOCATION, DESIGN, LIGHT REFLECTIVE VALUE AND TEXTURE MAY BE FOUND IN THE CURRENT EDITION OF THE TEXAS ACCESSIBILITY STANDARDS (TAS) AND 16 TAC 68.102.
 8. AT INTERSECTIONS WHERE CROSSWALK MARKINGS ARE NOT REQUIRED, CURB RAMP SHALL BE ALIGNED WITH THEORETICAL CROSSWALKS OR AS DIRECTED BY THE ENGINEER.
 9. PROVIDE A SMOOTH TRANSITION WHERE THE CURB RAMP CONNECTS TO THE STREET.
 10. FLARE SLOPE SHALL NOT EXCEED 10% MEASURED ALONG CURB LINE.
 11. ALL RAMP AND LANDINGS WITH A CONCRETE SURFACE SHALL HAVE A COURSE BROOM FINISH OR OTHER ROUGH NON-SKID TYPE FINISH AS APPROVED BY THE ENGINEER.
 12. PLACE 6" OF CONCRETE IN RAMP, LANDINGS, AND FLARES THAT ARE LOCATED AT THE RETURNS ADJACENT TO THE BACK OF CURB OF ARTERIAL STREETS AND COMMERCIAL SITES.
- GENERAL NOTES:**
1. CONCRETE TO BE CITY OF MIDLAND CLASS "A" WITH A MINIMUM 28 DAY COMPRESSION STRENGTH OF 3000 PSI. USE FIBER REINFORCED CONCRETE THROUGHOUT.
 2. MATERIALS AND CONSTRUCTION METHODS TO CONFORM TO THE CITY OF MIDLAND STANDARDS AND SPECIFICATIONS UNLESS OTHERWISE DIRECTED BY ADA STANDARDS AND RESOURCES.



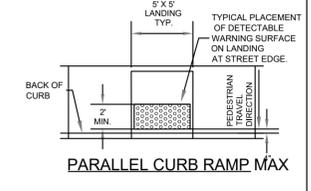
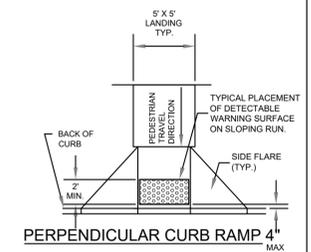
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CHECKED: JCF			
APPROVED: MCC			

MIDLAND Engineering Services

DIRECTIONAL RAMP WITHIN RADIUS

- CURB RAMP NOTES:**
1. ADJUST CURB RAMP LOCATION AND OR TYPE SO THAT NO OBSTRUCTION IS LOCATED WITHIN THE LANDING AREA.
 2. ALL SLOPES ARE MAXIMUM ALLOWABLE. THE LEAST POSSIBLE SLOPE THAT WILL STILL DRAIN PROPERLY SHOULD BE USED.
 3. LANDINGS SHALL BE 5' X 5' MINIMUM WITH A MAXIMUM 2% SLOPE IN ANY DIRECTION. WHERE OBSTRUCTIONS EXIST, THERE SHALL BE A 3' MINIMUM CLEARANCE IN WIDTH AND LENGTH OF THE LANDING.
 4. MANEUVERING SPACE AT THE BOTTOM OF CURB RAMP SHALL BE A MINIMUM OF 4' X 4' WHOLLY CONTAINED WITHIN THE CROSSWALK AND TO THE ENTIRE OUTSIDE OF THE PARALLEL VEHICULAR TRAVEL PATH.
 5. MAXIMUM ALLOWABLE CROSS SLOPE ON SIDEWALK AND CURB RAMP SURFACES IS 2%.
 6. CURB RAMP WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. EITHER BECAUSE THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR BECAUSE THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED. OTHERWISE, PROVIDE FLARED SIDES.
 7. ADDITIONAL INFORMATION ON CURB RAMP LOCATION, DESIGN, LIGHT REFLECTIVE VALUE AND TEXTURE MAY BE FOUND IN THE CURRENT EDITION OF THE TEXAS ACCESSIBILITY STANDARDS (TAS) AND 16 TAC 68.102.
 8. AT INTERSECTIONS WHERE CROSSWALK MARKINGS ARE NOT REQUIRED, CURB RAMP SHALL BE ALIGNED WITH THEORETICAL CROSSWALKS OR AS DIRECTED BY THE ENGINEER.
 9. PROVIDE A SMOOTH TRANSITION WHERE THE CURB RAMP CONNECTS TO THE STREET.
 10. FLARE SLOPE SHALL NOT EXCEED 10% MEASURED ALONG CURB LINE.
 11. ALL RAMP AND LANDINGS WITH A CONCRETE SURFACE SHALL HAVE A COURSE BROOM FINISH OR OTHER ROUGH NON-SKID TYPE FINISH AS APPROVED BY THE ENGINEER.
 12. PLACE 6" OF CONCRETE IN RAMP, LANDINGS, AND FLARES THAT ARE LOCATED AT THE RETURNS ADJACENT TO THE BACK OF CURB OF ARTERIAL STREETS AND COMMERCIAL SITES.
- GENERAL NOTES:**
1. CONCRETE TO BE CITY OF MIDLAND CLASS "A" WITH A MINIMUM 28 DAY COMPRESSION STRENGTH OF 3000 PSI. USE FIBER REINFORCED CONCRETE THROUGHOUT.
 2. MATERIALS AND CONSTRUCTION METHODS TO CONFORM TO THE CITY OF MIDLAND STANDARDS AND SPECIFICATIONS UNLESS OTHERWISE DIRECTED BY ADA STANDARDS AND RESOURCES.

- DETECTABLE WARNING SURFACE NOTES:**
1. DETECTABLE WARNING SURFACE SUCH AS ARMOR-TILE ADA SOUND AMPLIFYING DETECTABLE/TACTILE WARNING SURFACE TILE OR EQUAL AND APPROVED BY THE ENGINEER AS MEETING ALL REQUIREMENTS OF ASTM C-936, C-33.
 2. CURB RAMP MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSISTS OF RAISED TRUNCATED DOMES COMPLYING WITH SECTION 7.05 DETACHABLE WARNINGS OF THE TEXAS ACCESSIBILITY STANDARDS (TAS). THE SURFACE MUST CONTRAST VISUALLY WITH ADJOINING SURFACES, INCLUDING SIDE FLARES. PURNISH DARK BROWN OR DARK RED DETECTABLE WARNING SURFACE ADJACENT TO UNCOLORED CONCRETE UNLESS SPECIFIED ELSEWHERE IN THE PLANS.
 3. DETECTABLE WARNING SURFACES MUST BE SLIP RESISTANT AND NOT ALLOW WATER TO ACCUMULATE.
 4. ALIGN TRUNCATED DOMES IN THE DIRECTION OF PEDESTRIAN TRAVEL WHEN ENTERING THE STREET.
 5. SHADED AREAS INDICATE THE APPROXIMATE LOCATION FOR THE DETECTABLE WARNING SURFACE FOR EACH CURB RAMP TYPE.
 6. DETECTABLE WARNING SURFACES SHALL BE A MINIMUM OF 2" IN DEPTH IN THE DIRECTION OF PEDESTRIAN TRAVEL.
 7. DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS A MINIMUM OF 6" AND A MAXIMUM OF 10" FROM THE EXTENSION OF THE FACE OF CURB. DETECTABLE WARNING SURFACES MAY BE CURVED ALONG THE CORNER RADIUS.
 8. SINGLE TILE PLATES COLONIAL RED COLOR.
- GENERAL NOTES:**
1. CONCRETE TO BE CITY OF MIDLAND CLASS "A" WITH A MINIMUM 28 DAY COMPRESSION STRENGTH OF 3000 PSI. USE FIBER REINFORCED CONCRETE THROUGHOUT.
 2. MATERIALS AND CONSTRUCTION METHODS TO CONFORM TO THE CITY OF MIDLAND STANDARDS AND SPECIFICATIONS UNLESS OTHERWISE DIRECTED BY ADA STANDARDS AND RESOURCES.



DRAWN: DPM	EFFECTIVE DATE: 10/01/2018	SCALE: NTS	DETAIL: 233
CHECKED: JCF			
APPROVED: MCC			

MIDLAND Engineering Services

DETECTABLE WARNING SURFACE

Parkhill



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Cholla Road and County Road 1250
Paving Improvements

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Midland County

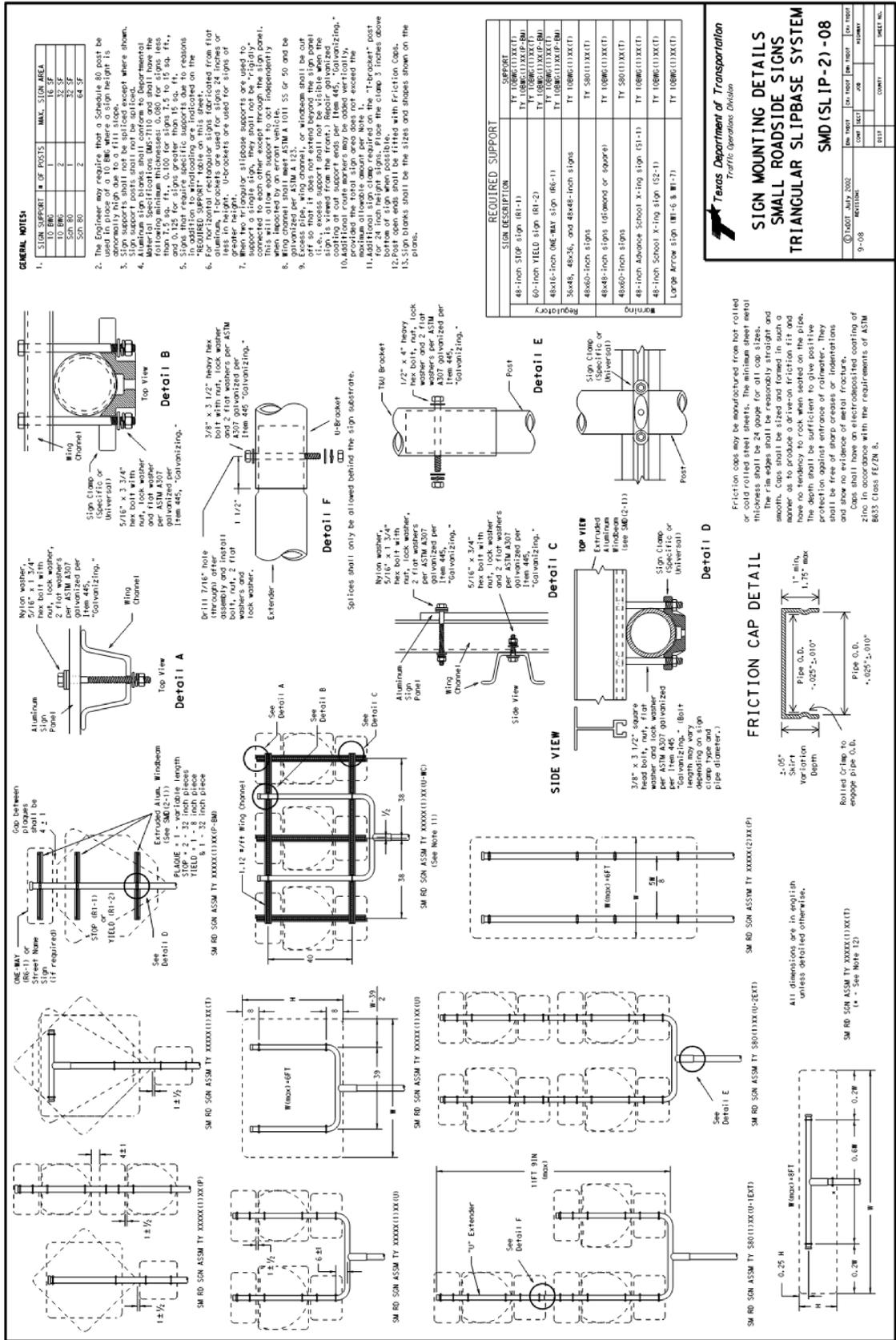
PROJECT NO.
6000.20

2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection

DATE DESCRIPTION

Details
26

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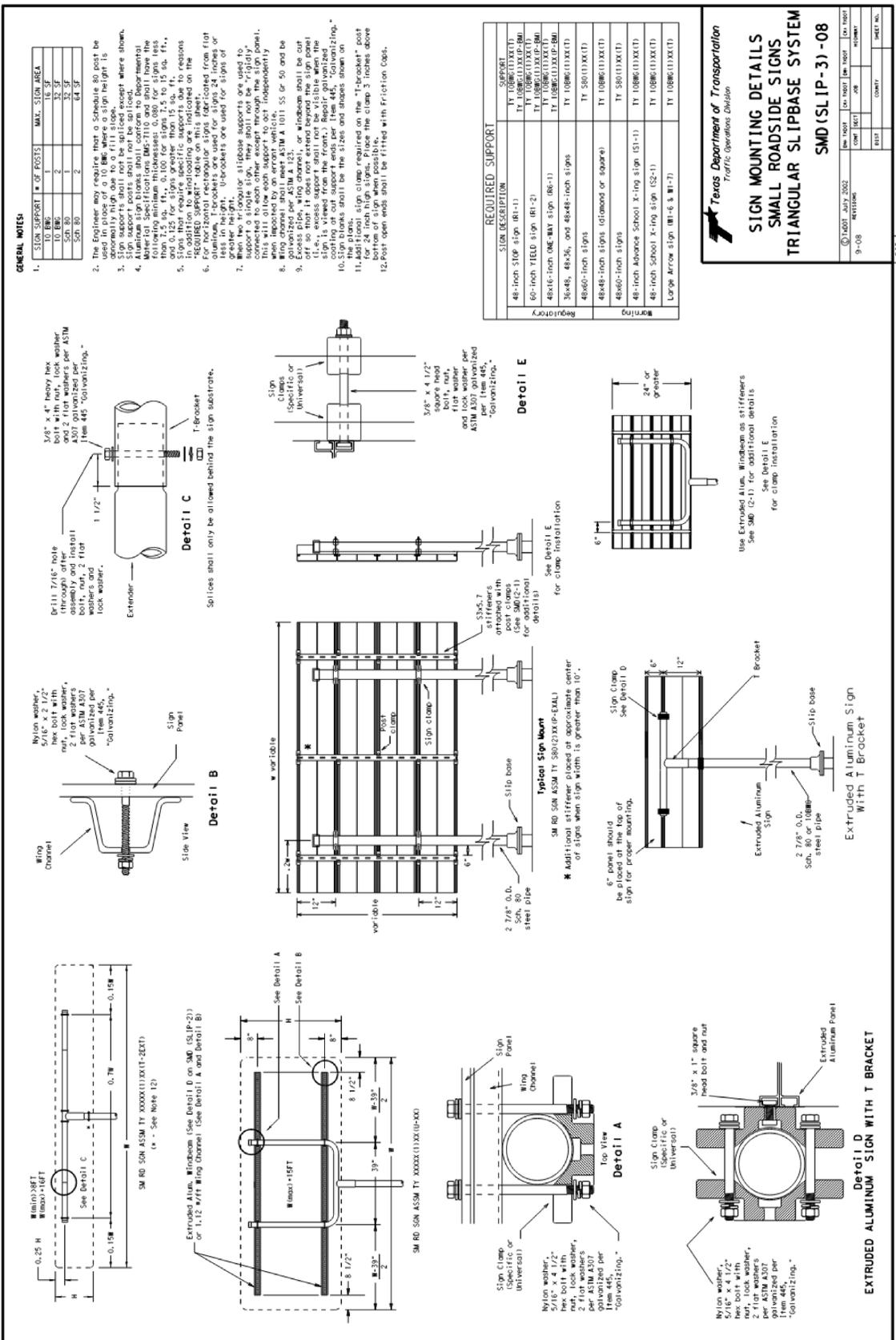


DATE: 03/19/2021 BY: KCHRISTOPHER FOR: MIDLAND COUNTY PROJECT: 6000-20 SHEET NO. 28

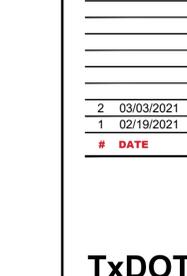


Texas Department of Transportation Traffic Operations Division

SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS TRIANGULAR SLIPBASE SYSTEM SMD (SLIP-2) - 08



DATE: 03/19/2021 BY: KCHRISTOPHER FOR: MIDLAND COUNTY PROJECT: 6000-20 SHEET NO. 28



Texas Department of Transportation Traffic Operations Division

SIGN MOUNTING DETAILS SMALL ROADSIDE SIGNS TRIANGULAR SLIPBASE SYSTEM SMD (SLIP-3) - 08

Parkhill

PARKHILL SMITH & COOPER, INC. P.O. Box 509
 131920
 KRISTY R. CHRISTOPHER
 03/03/2021

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Cholla Road and County Road 1250
 Paving Improvements

CLIENT: Midland County

PROJECT NO.: 6000-20

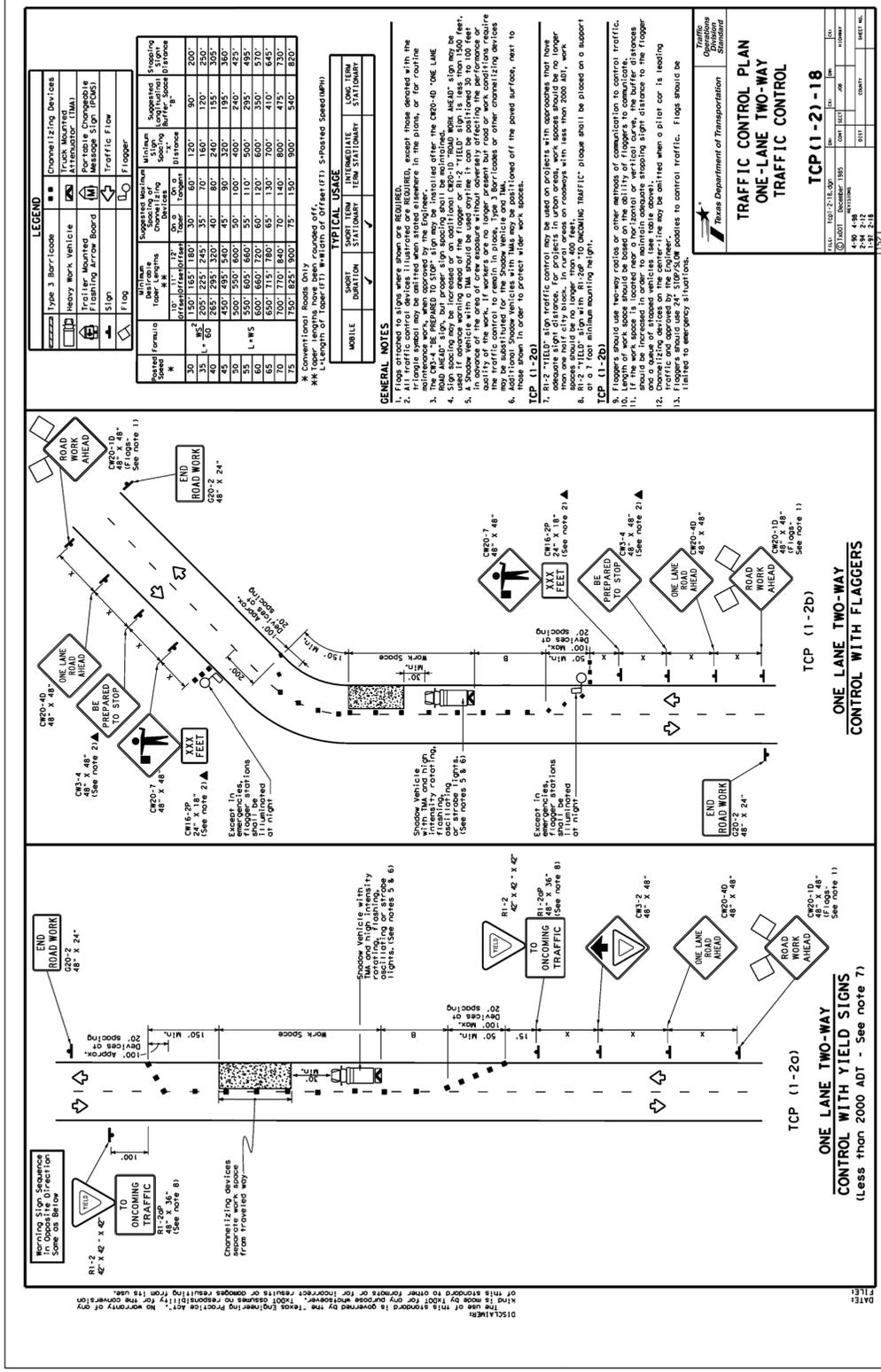
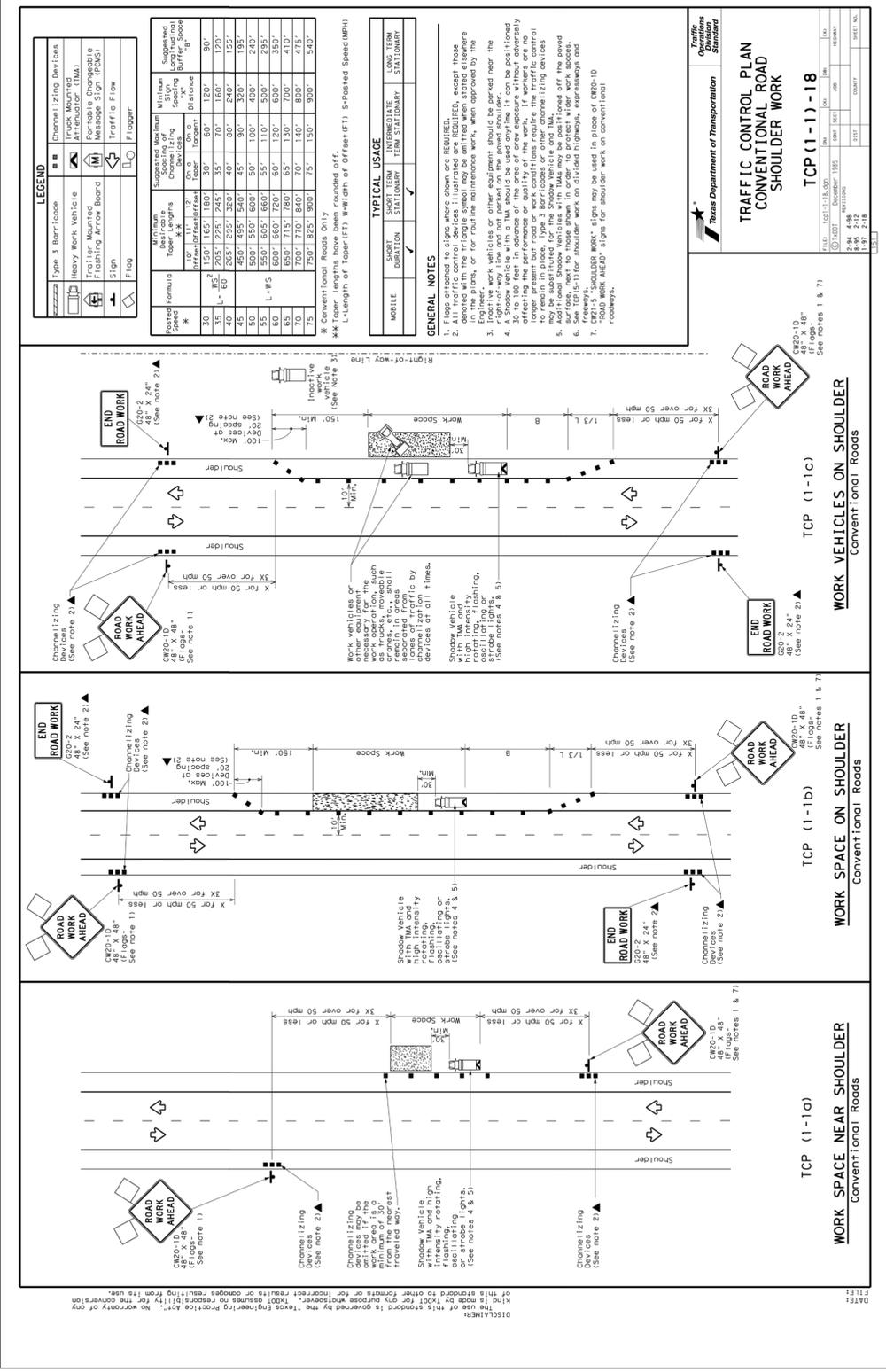
2 03/03/2021 Response to Comments
 1 02/19/2021 Waterline Projection

DATE DESCRIPTION

TxDOT Sign Mounting Details

28

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CLIENT
Midland County

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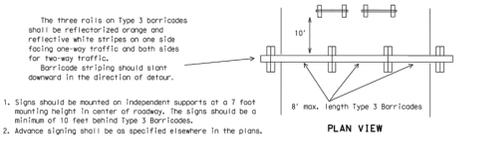
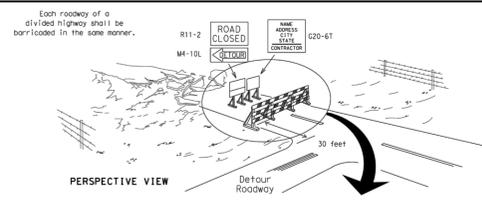
PARKHILL SMITH & COOPER, INC. F-5389
KRYSTY R. CHRISTOPHER
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03/03/2021

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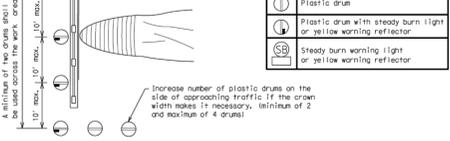
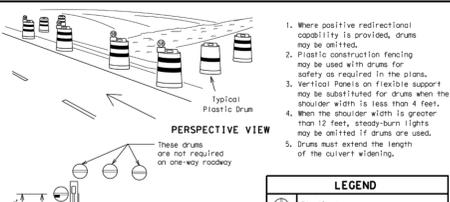
Cholla Road and County Road 1250
Paving Improvements

TYPE 3 BARRICADES

1. Refer to the Compliant Work Zone Traffic Control Devices List (CWZTDL) for details of the Type 3 Barricades and a list of all materials used in the construction of Type 3 Barricades.
2. Type 3 Barricades shall be used at each end of construction projects closed to all traffic.
3. Barricades extending across a roadway should have stripes that slope downward in the direction toward which traffic must turn in detouring. When both right and left turns are provided, the chevron striping may slope downward in both directions from the center of the barricade. Where no turns are provided at a closed road striping should slope downward in both directions toward the center of roadway.
4. Striping of rolls, for the right side of the roadway, should slope downward to the left; for the left side of the roadway, striping should slope downward to the right.
5. Identification markings may be shown only on the back of the barricade rolls. The maximum height of letters and/or company logos used for identification shall be 1".
6. Barricades shall not be placed parallel to traffic unless an adequate clear zone is provided.
7. Warning lights shall NOT be installed on barricades.
8. Where barricades require the use of weights to keep from turning over, the use of sandbags with dry, cohesionless sand is recommended. The sandbags will be tied shut to keep the sand from spilling and to maintain a constant weight. Sand cones shall not be used in a manner that covers any portion of a barricade roll's reflective sheeting. Rocks, concrete, lime, steel or other solid objects will NOT be permitted. Sandbags should weigh a minimum of 35 lbs and a maximum of 50 lbs. Sandbags shall be made of a durable material that tears upon vehicular impact. Rubber (such as tire inner tubes) shall not be used for sandbags. Sandbags shall only be placed atop or upon the base supports of the device and shall not be suspended above ground level or hung with rope, wire, chains or other fasteners.
9. Sheet piling for barricades shall be retroreflective Type A conforming to Departmental Material Specification DMS-8300 unless otherwise noted.



TYPE 3 BARRICADE (POST AND SKID) TYPICAL APPLICATION

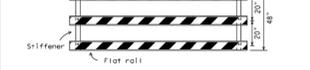


CULVERT WIDENING OR OTHER ISOLATED WORK WITHIN THE PROJECT LIMITS

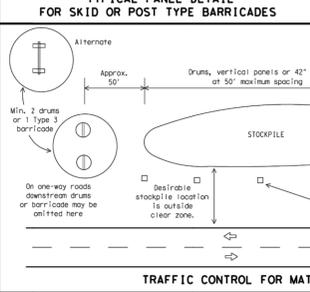
Barricades shall NOT be used as a sign support.



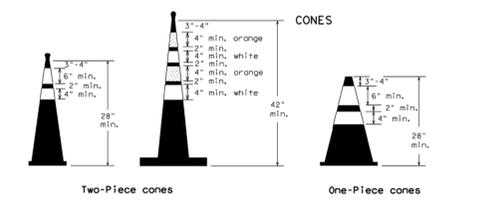
TYPICAL STRIPING DETAIL FOR BARRICADE RAIL



TYPICAL PANEL DETAIL FOR SKID OR POST TYPE BARRICADES



TRAFFIC CONTROL FOR MATERIAL STOCKPILES

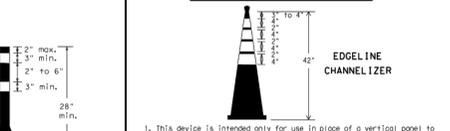


Two-Piece cones One-Piece cones Tubular Marker

28" Cones shall have a minimum weight of 9 1/2 lbs. 42" 2-piece cones shall have a minimum weight of 30 lbs. including base.

1. Traffic cones and tubular markers shall be predominantly orange, and meet the height and weight requirements shown above.
2. One-piece cones have the body and base of the cone molded in one consolidated unit. Two-piece cones have a cone shaped body and a separate rubber base, or ballast, that is added to keep the device upright and in place.
3. Two-piece cones may have a handle or loop extending up to 8" above the minimum height shown, in order to aid in retrieving the device.
4. Cones or tubular markers used at night shall have white or white and orange reflective bands as shown above. The reflective bands shall have a smooth, sealed outer surface and meet the requirements of Departmental Material Specification DMS-8300 Type A.
5. 28" cones and tubular markers are generally suitable for short duration and short-term stationary work as defined on BC(14). These should not be used for intermediate-term or long-term stationary work unless personnel is on-site to maintain them in their proper upright position.
6. 42" two-piece cones, vertical panels or drums are suitable for all work zone applications.
7. Cones or tubular markers used on each project should be of the same size and shape.

THIS DEVICE SHALL NOT BE USED ON PROJECTS LET AFTER MARCH 2014.



EDGELINE CHANNELIZER

1. This device is intended only for use in place of a vertical panel to channelize traffic by indicating the edge of the travel lane. It is not intended to be used in transitions or tapered.
2. This device shall not be used to separate lanes of traffic (capping or otherwise) or warn of objects.
3. This device is based on a 42 inch, two-piece cone with an alternate striping pattern: four 4 inch retroreflective bands, with an approximate 2 inch gap between bands. The color of the band should correspond to the color of the edgeline (yellow for left edgeline, white for right edgeline) for which the device is substituted or for which it supplements. The reflective bands shall be retroreflective Type A conforming to Departmental Material Specification DMS-8300, unless otherwise noted.
4. The base must weigh a minimum of 30 lbs.

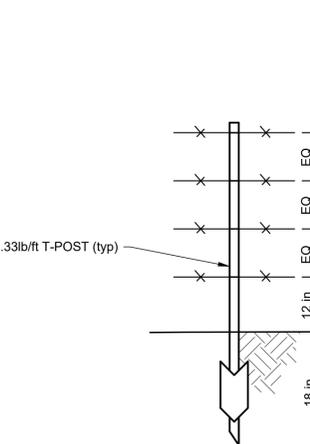
SHEET 10 OF 12

Texas Department of Transportation Traffic Operations Division Standard

BARRICADE AND CONSTRUCTION CHANNELIZING DEVICES

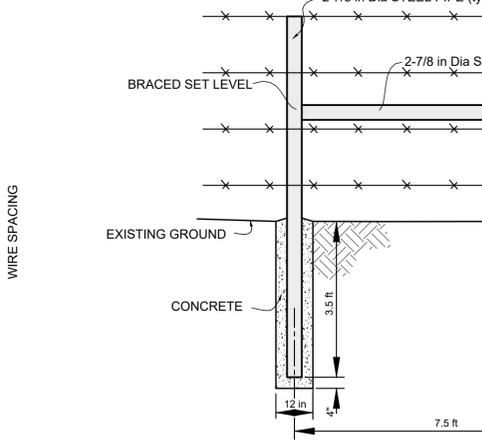
BC(10)-14

File:	bc-14.dgn	Rev:	TxDOT	Rev:	TxDOT	Rev:	TxDOT	Rev:	TxDOT
Created:	November 2002	Drawn:	REK	Checked:	REK	Job:		Sheet:	
Revised:	9-07 8-14	Dist:		County:		Project:		Sheet No.:	
	7-13								



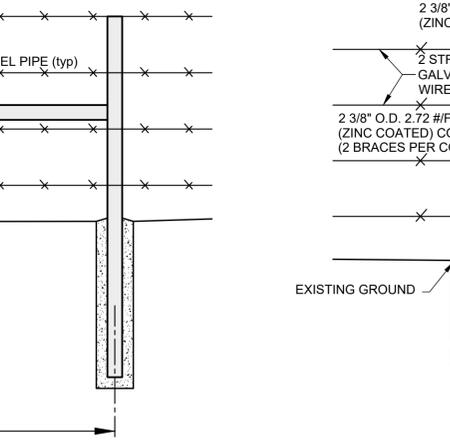
B1 WIRE SPACING AND T-POST

NO SCALE



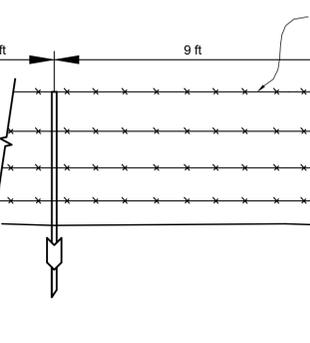
B2 H-POST AND CORNER POST

NO SCALE



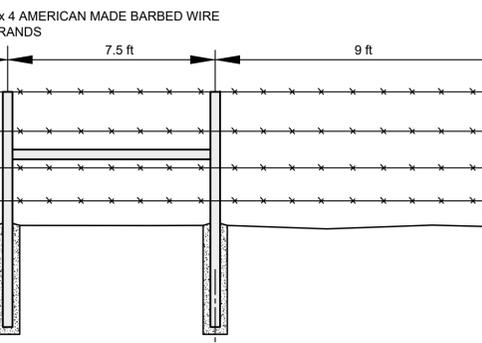
EXISTING GROUND

2'-0" 1'-0" 4"



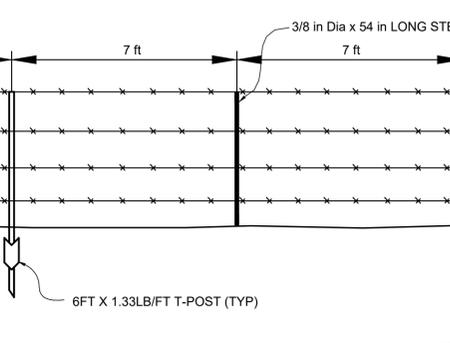
A1 TYPICAL BARBED WIRE FENCE

NO SCALE



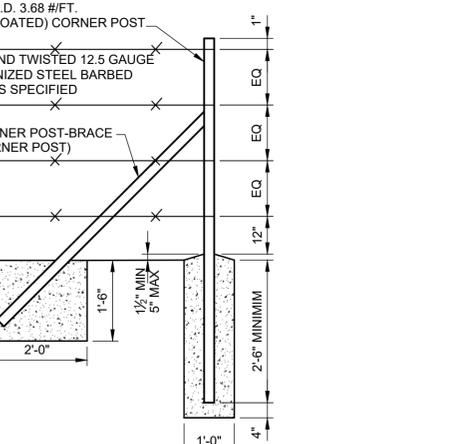
NOTE:

ALL STEEL POSTS AND STAYS TIED IN WITH 14 ga. TIE WIRE, T-POST FASTENED WITH T-POST LIPS, ON EACH WIRE, ON EACH POST.



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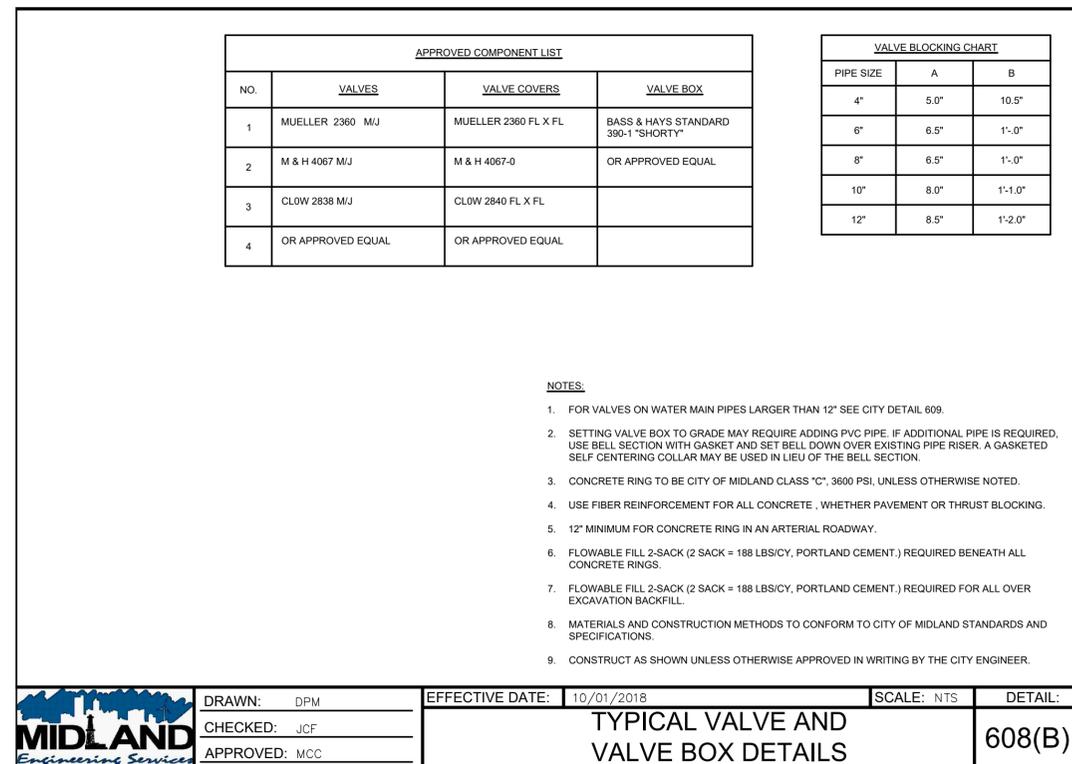
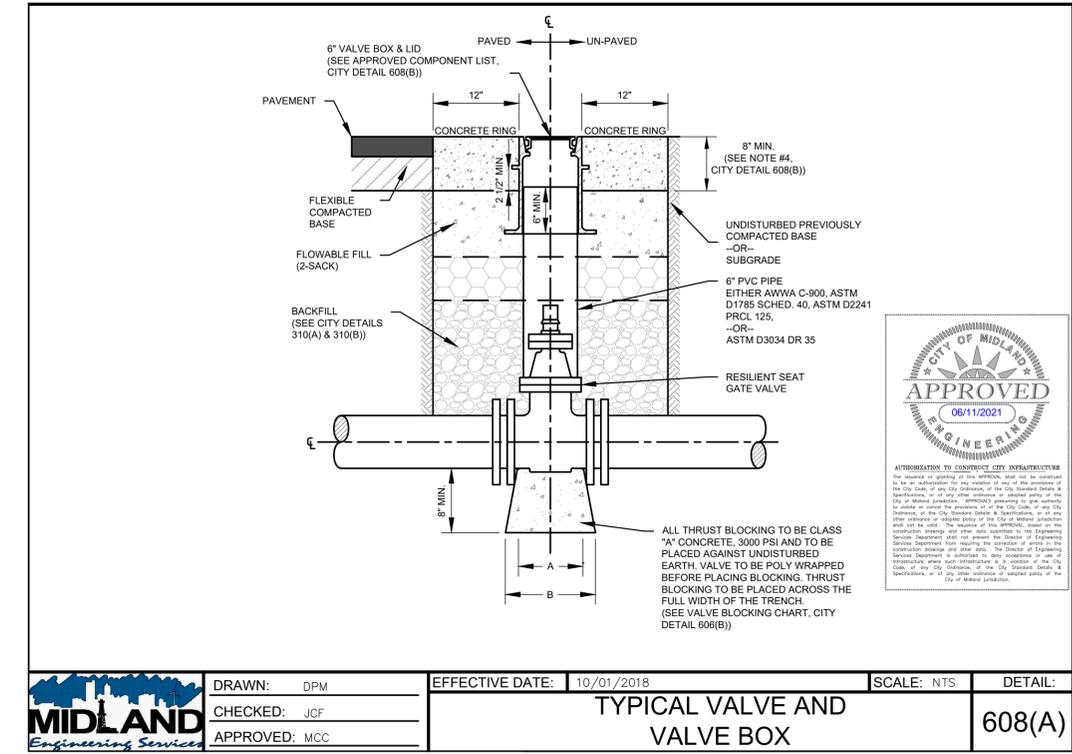
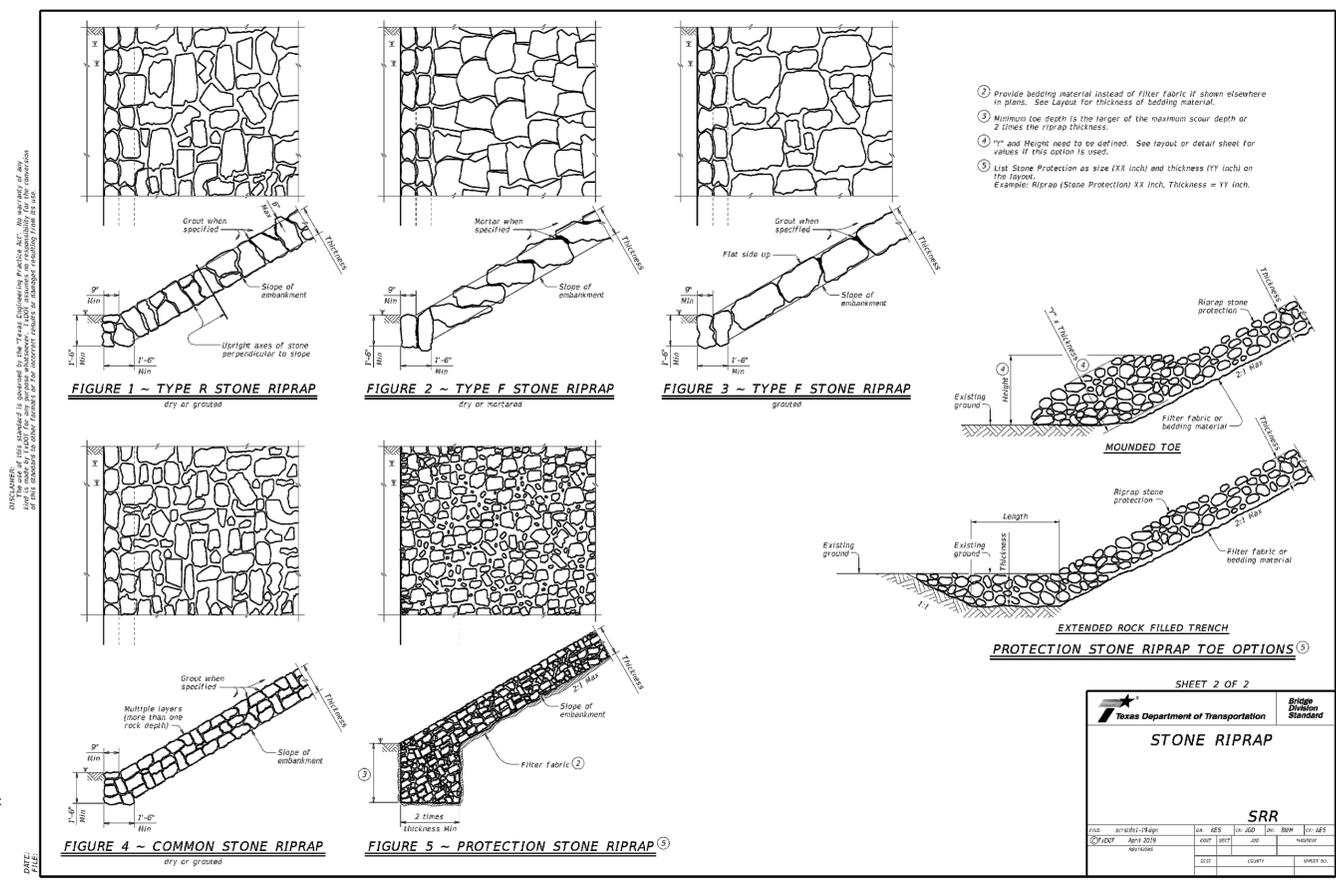
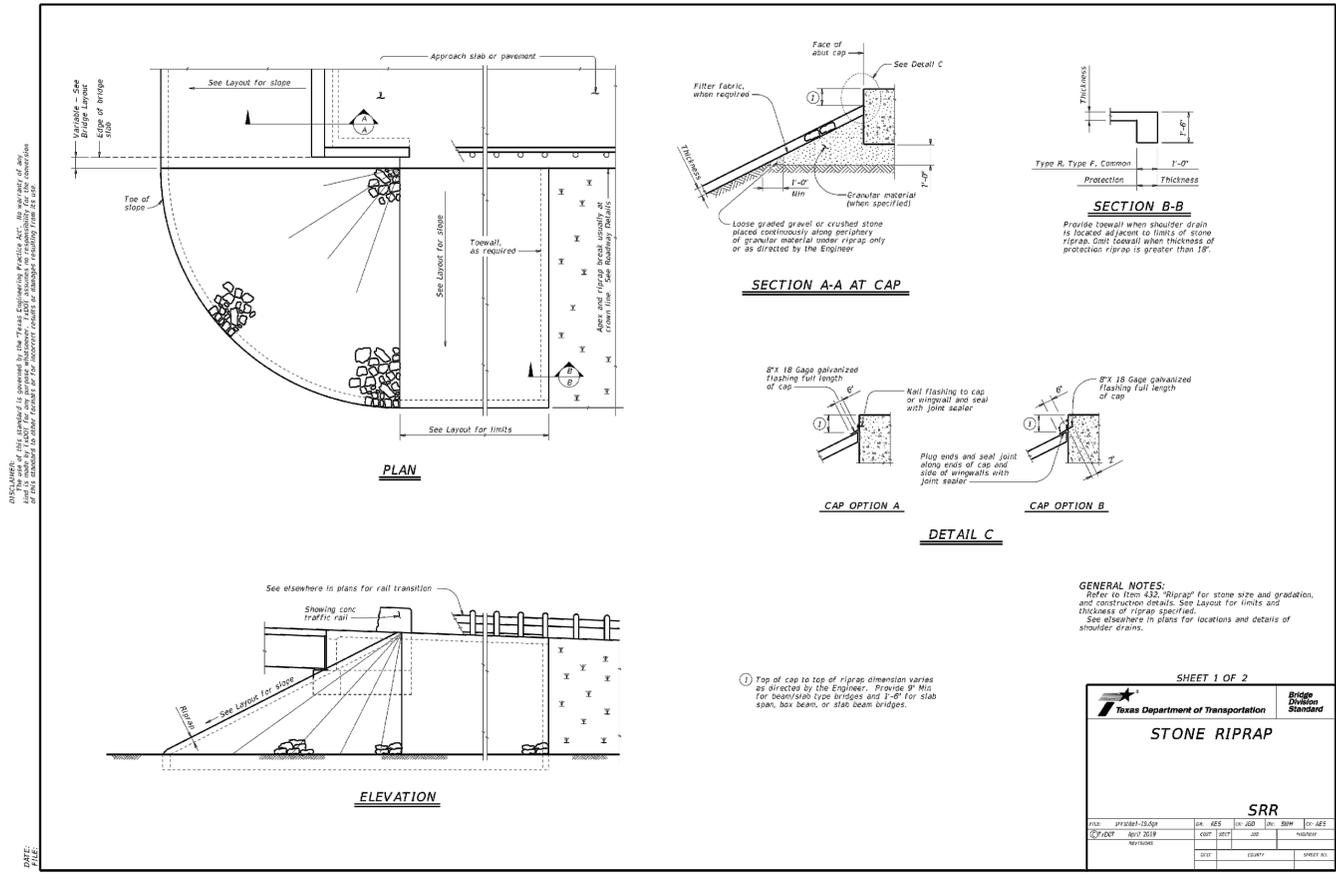


NOTE:

ALL STEEL POSTS AND STAYS TIED IN WITH 14 ga. TIE WIRE, T-POST FASTENED WITH T-POST LIPS, ON EACH WIRE, ON EACH POST.



4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection



#	DATE	DESCRIPTION
5	06/10/2021	Addendum 1
4	04/16/2021	Midland County Comments
3	03/17/2021	Response to 2nd Comments
2	03/03/2021	Response to Comments
1	02/19/2021	Waterline Projection